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## CITY PLANS PANEL

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Meeting to be held in Civic Hall, Leeds on  
Thursday, 2nd November, 2017 at 1.30 pm

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### MEMBERSHIP

#### Councillors

P Gruen  
N Walshaw  
J McKenna  
(Chair)  
A Khan  
A Garthwaite  
B Selby  
C Macniven  
E Nash

C Campbell

R Procter  
G Latty

T Leadley

D Blackburn

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**Agenda compiled by:**  
**John Grieve**  
**Governance Services**  
**Civic Hall**  
**Tel: 0113 37 88662**

# A G E N D A

Item No	Ward	Item Not Open		Page No
1			<p data-bbox="678 322 975 356"><b>SITE VISIT LETTER</b></p> <p data-bbox="678 465 1401 533"><b>APPEALS AGAINST REFUSAL OF INSPECTION OF DOCUMENTS</b></p> <p data-bbox="678 613 1382 757">To consider any appeals in accordance with Procedure Rule 15.2 of the Access to Information Rules (in the event of an Appeal the press and public will be excluded)</p> <p data-bbox="678 797 1385 940">(*In accordance with Procedure Rule 15.2, written notice of an appeal must be received by the Head of Governance Services at least 24 hours before the meeting)</p>	

Item No	Ward	Item Not Open		Page No
2			<p><b>EXEMPT INFORMATION - POSSIBLE EXCLUSION OF PRESS AND PUBLIC</b></p> <p>1 To highlight reports or appendices which officers have identified as containing exempt information, and where officers consider that the public interest in maintaining the exemption outweighs the public interest in disclosing the information, for the reasons outlined in the report.</p> <p>2 To consider whether or not to accept the officers recommendation in respect of the above information.</p> <p>3 If so, to formally pass the following resolution:-</p> <p><b>RESOLVED</b> – That the press and public be excluded from the meeting during consideration of the following parts of the agenda designated as containing exempt information on the grounds that it is likely, in view of the nature of the business to be transacted or the nature of the proceedings, that if members of the press and public were present there would be disclosure to them of exempt information, as follows:-</p>	
3			<p><b>LATE ITEMS</b></p> <p>To identify items which have been admitted to the agenda by the Chair for consideration</p> <p>(The special circumstances shall be specified in the minutes)</p>	
4			<p><b>DECLARATIONS OF DISCLOSABLE PECUNIARY INTERESTS</b></p> <p>To disclose or draw attention to any disclosable pecuniary interests for the purposes of Section 31 of the Localism Act 2011 and paragraphs 13-16 of the Members’ Code of Conduct.</p>	

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5			<p><b>APOLOGIES FOR ABSENCE</b></p> <p>To receive apologies for absence (If any)</p>	
6			<p><b>MINUTES OF THE PREVIOUS MEETING</b></p> <p>To consider and approve the minutes of the previous meeting held on 12<sup>th</sup> October 2017.</p> <p>(Copy attached)</p>	3 - 18
7			<p><b>MATTERS ARISING</b></p> <p>To consider any matters arising from the minutes of the previous meeting.</p>	
8	City and Hunslet		<p><b>APPLICATION NO.17/02666/FU - TEN STOREY BLOCK OF 101 APARTMENTS WITH GROUND FLOOR RESIDENTS LOUNGE, CINEMA ROOM, GYM AND CYCLE STORAGE ROOM AT 16-18 MANOR ROAD, HOLBECK, LEEDS LS11 9AH</b></p> <p>To consider a report by the Chief Planning Officer which sets out details of an application for a ten storey block of 101 apartments with ground floor residents lounge, cinema room, gym and cycle storage room at 16-18 Manor Road, Holbeck, Leeds LS11 9AH.</p> <p>(Report attached)</p>	19 - 42
9	City and Hunslet		<p><b>PREAPP17/00517 - PRE-APPLICATION PRESENTATION FOR RESIDENTIAL DEVELOPMENT, ASSOCIATED FACILITIES WITH LINKED PODIUM AT DONCASTER MONK BRIDGE WHITEHALL ROAD, LOWER WORTLEY LEEDS LS12 1BE</b></p> <p>To consider a report by the Chief Planning Officer which sets out details of a pre-application presentation for residential development, associated facilities with linked podium At Doncaster Monk Bridge Whitehall Road, Lower Wortley Leeds LS12 1BE.</p> <p>(Report attached)</p>	43 - 60

Item No	Ward	Item Not Open		Page No
10			<p><b>DATE AND TIME OF NEXT MEETING</b></p> <p>To note that the next meeting will take place on Thursday, 23<sup>rd</sup> November 2017 at 1.30pm in the Civic Hall, Leeds.</p>	

**Third Party Recording**

Recording of this meeting is allowed to enable those not present to see or hear the proceedings either as they take place (or later) and to enable the reporting of those proceedings. A copy of the recording protocol is available from the contacts named on the front of this agenda.

Use of Recordings by Third Parties– code of practice

- a) Any published recording should be accompanied by a statement of when and where the recording was made, the context of the discussion that took place, and a clear identification of the main speakers and their role or title.
- b) Those making recordings must not edit the recording in a way that could lead to misinterpretation or misrepresentation of the proceedings or comments made by attendees. In particular there should be no internal editing of published extracts; recordings may start at any point and end at any point but the material between those points must be complete.

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To all Members of City Plans Panel

**Planning Services**  
The Leonardo Building  
2 Rossington Street  
Leeds  
LS2 8HD

Contact: Daljit Singh  
Tel: 0113 3787971  
daljit.singh@leeds.gov.uk

Our ref: City Site Visits  
Date: 20.10.2017

Dear Councillor

**SITE VISITS – CITY PLANS PANEL – THURSDAY 2<sup>nd</sup> November 2017**

Prior to the meeting of City Plans Panel on Thursday 2<sup>nd</sup> November 2017 the following site visits will take place.

Time	Ward	Site
10.15 - 10.45am	City & Hunslet	17/02666/FU –16-18 Manor Road, Leeds 11
11.00 - 11.30am	City & Hunslet	PREAPP/17/00517 – Former Doncaster Monkbridge site, Whitehall Road

Please notify Daljit Singh (Tel: 3787971) if you will be attending and meet in the Ante Chamber at **9.55 am at the latest for a prompt start at 10am.**

Yours sincerely

Daljit Singh  
Central Area Team Leader

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## CITY PLANS PANEL

THURSDAY, 12TH OCTOBER, 2017

**PRESENT:** Councillor J McKenna in the Chair

Councillors P Gruen, D Blackburn, G Latty,  
T Leadley, N Walshaw, C Campbell,  
B Selby, C Macniven, E Nash, C Towler  
and B Anderson

Members site visits were held in connection with the following proposals:  
Application No. 17/02501/OT – Former Tetley Brewery Site, Hunslet Road,  
Leeds 10 and PREAPP/17/00604 – 4 – 32 George Street, Leeds 2 and was  
attended by the following Councillors: P Gruen, J McKenna, C Campbell,  
G Latty, T Leadley and D Blackburn.

### **51 Chair's Opening Comments**

The Chair welcomed to the meeting John Thorp, former City Architect who had been the visionary in shaping the City Centre including the South Bank area.

### **52 Appeals Against Refusal of Inspection of Documents**

There were no appeals against the refusal of inspection of documents.

### **53 Exempt Information - Possible Exclusion of Press and Public**

There were no items identified where it was considered necessary to exclude the press or public from the meeting due to the nature of the business to be considered.

### **54 Late Items**

There were no late items.

### **55 Declarations of Disclosable Pecuniary Interests**

Although not a disclosable pecuniary interest Councillors: J McKenna, C Macniven and B Anderson required it to be recorded that they had an interest in item No.11 (Pre Application Presentation for a 6 storey Apart-Hotel with ground floor commercial uses and accesses to Leeds City Market at 4 -32 George Street, Leeds 2) because they were Members of the Kirkgate Market Board and had participated in meetings at which the proposals had been discussed (Minute No.61 referred)

### **56 Apologies for Absence**

Apologies for absence were received from Councillors: A Garthwaite,

Draft minutes to be approved at the meeting  
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A Khan and R Procter.

Councillors: B Anderson and C Towler were in attendance as substitutes.

## **57 Minutes of the Previous Meeting**

The Minutes of the previous meeting held on 21<sup>st</sup> September 2017 were submitted for consideration and approval.

Councillor Leadley requested a minor amendment to Minute No. 49 removing the word “required” and replacing with “welcomed”.

**RESOLVED** – That with the inclusion of the above, the minutes of the previous meeting held on 21<sup>st</sup> September 2017 were accepted as a true and correct record.

## **58 Matters Arising**

There were no issues raised under matters arising.

## **59 Application No. 17/02501/OT - Outline application (all matters reserved except for access) for a phased mixed use development comprising demolition of existing buildings, up to 850 residential units (C3), business uses (B1), flexible commercial uses (A1, A2, A3, A4, A5, B1, D1, D2), hotel use (C1), public realm including a City Park and vehicular access at Former Tetley Brewery, Hunslet Road, Hunslet, Leeds LS10 1JQ**

The Chief Planning Officer submitted a report which set out details of an outline application (all matters reserved except for access) for a phased mixed use development comprising demolition of existing buildings, up to 850 residential units (C3), business uses (B1), flexible commercial uses (A1, A2, A3, A4, A5, B1, D1, D2), hotel use (C1), public realm including a City Park and vehicular access at Former Tetley Brewery, Hunslet Road, Hunslet, Leeds LS10 1JQ.

Site photographs and plans were displayed and referred to throughout the discussion of the application.

Members were made aware that Highways England had placed a holding direction on the application and had requested work to be undertaken to understand the traffic impact of the proposed development on the motorway network, determine if any mitigation was necessary and what that might be. Members noted that the Local Planning Authority could not determine the application until such time the outcome of the requested work was understood and the holding direction had been lifted.

The Planning Case Officer addressed the Panel, speaking in detail about the proposal and highlighted the following:

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- The outline application related to the redevelopment of the area of land including Crown Point Road to the east, Waterloo Street and Bowman Lane to the north, Meadow Lane to the west and Hunslet Road (A61) to the south, a total area of 9.7 hectares. 6.69 hectares of the defined application boundary is land within the applicant's ownership.
- The second part (1.98ha) of the applicant's ownership does not form part of this application and is the area between Crown Point Road, Black Bull Street and Hunslet Road (A61). Phase 2 will be the subject of a future planning application.
- The site is located in the City Centre as designated in the Development Plan. It is also within the adopted South Bank Planning Statement 2011 and emerging Aire Valley Leeds Area Action Plan areas. The site has been under-utilised since 2011 when the brewery closed, and most of the brewery buildings were demolished. The site is currently in use as a temporary car park, temporary greenspace, temporary training facility for ASDA, and an art hub at The Tetley building.
- The proposal has potential to contribute to planned new housing and jobs provision and place-making opportunities for the South Bank/Aire Valley area.
- The site is currently unallocated in the saved LDF Proposals Map, but is allocated for mixed use including residential in the emerging Aire Valley Area Action Plan.
- The Tetley building and gatehouse are unlisted heritage assets in the phase one application site boundary. Outside the phase one site, the Grade II listed Salem Chapel lies to the west, between Hunslet Road and Hunslet Lane, and the Grade II\* listed Chadwick Lodge is to the east of the site at the junction of Crown Point Road and Black Bull Street.
- The surrounding area features a wide range of existing land uses. To the north of the site along Bowman Lane and Waterloo Street are residential blocks ranging between 5 and 13 storeys including those at Brewery Wharf. To the east lies the mixed use residential development at Leeds Dock, and the education hub of Leeds College of Building, Ruth Gorse Academy, Leeds City College and Leeds University Technical College (UTC). To the south is a mixture of retail at Crown Point Retail Park, and offices at Leeds City Office Park. To the west lies the Asda office headquarters, New Lane office park and the River Aire corridor.
- The proposal is an outline planning application (all matters reserved except for access) for a phased mixed use development comprising

demolition of existing buildings, up to 850 residential units (C3), business uses (B1), flexible commercial uses (A1, A2, A3, A4, A5, B1, D1, D2), hotel use (C1), public realm including a City Park and vehicular access.

- This would include up to 85000 sqm offices, 15000 sqm flexible ground floor units and up to two hotels with a combined total of 400 beds (use class C1)
- Vehicular accesses are proposed on Crown Point Road, Great Wilson Street, Meadow Lane, Waterloo Street and Hunslet Road. Two main tree-lined access roads; Street 'X' and Street 'Y' are proposed with car parking up to 860 spaces (including 83 disabled parking spaces) in four basements.
- City Park - A minimum of 2 hectare of public realm as a contribution to the City Park (30% of the applicant's ownership within the application site area) plus additional public routes, semi-private and private open spaces
- The applicants propose an indicative layout and scale of buildings and spaces in support of their outline planning application for this first phase. This would be formed in 8 principal blocks ranging in height from 8 – 39 storey's (storey heights are approximate based on 3m storey height residential and hotel, and 4m storey heights for offices and other commercial use classes):
- The Flexible Commercial Uses would be accommodated at ground and upper levels in order to provide active frontages including to the proposed City Park and two new proposed east/west streets.
- The City Centre Park, created by a series of phased linked "stepping stones" as first identified in the South Bank Planning Statement 2011, is an important aspect of the applicant proposals. As envisaged by the Council's adopted vision, the Park would be formed over time by an area of land starting along the River Aire embankment in the west, sweeping across the former Tetley brewery site, giving over 3.5 hectare of greenspace made up of linked elements, including the Council's own land in Meadow Lane, and extending to the south east across Crown Point Road. The park would be in an arc-shape across the former brewery site, principally running on a north-west to south-east orientation via a central space at The Tetley, with smaller areas of greenspace and courtyards between buildings, including a new greenspace facing Bowman Lane. Detailed allocation of functions such as events, play, recreation, landscape design and planting proposals for the park would form part of future reserved matters application(s).

In response to Members questions, the following issues were discussed:

- Members welcomed the principle of the application, it was accepted that the application before Members was for the outline stage but there was a lack of substance, could further details be provided.
- It appeared that Members were been asked to “approve a leap of faith” could examples be provided of work done elsewhere or possibly a site visit to understand what was planned for the whole of the site
- The proposed courtyard area appeared to face northwest, would sunlight be able to penetrate into this area.
- It was noted that parameter plans and highways access drawings had been provided but what other studies had been undertaken
- How would CO2 emissions and noise reductions be achieved
- The demand for residential accommodation in this area was likely to be high but the demand for hotel accommodation may not be as great, is it possible the scheme could be amended as a result
- Was it intended that all the properties would be sold
- There was already a lot of office space within the city centre, both old and new, did this scheme deliver the right balance
- Members welcomed the proposal for a City Park but it appeared to be split into two sections, could more greenspace/ connectivity be provided to give the impression of being in a park
- Were there any proposals for public transport for the area given the fact that the temporary car park would be closing
- Had there been any discussions with other developers in the area about predicated traffic distribution/ circulation, had any studies been undertaken
- Had any discussions taken place about the “Late Night Economy” late night café bars/ restaurants, convenience stores and public transport
- Currently the site had 800 car parking spaces, where would these cars go once development of the site began

In responding to the issues raised, the Planning Case Officer together with the applicant’s representative provided the following responses:

- In terms of lack of detail, it was emphasised that it was an outline application with access reserved before Members, the plans were indicative at this stage and full details would be provided at the Reserved Matters stage for each phase.
- Responding to the suggestion that Members undertake a visit to a similar in-progress scheme by the applicant, the applicants welcomed the suggestion commenting that a scheme in Stratford, London would provide an ideal opportunity to view a similar scheme. It was also suggested that a workshop could be facilitated as part of the visit to provide Members with a greater understanding of the future reserved matters phases.
- It was reported that the proposals for the courtyard areas were flexible and could be adapted to ensure sunlight penetration.
- On the question of what studies had been undertaken in preparation of the development, it was reported that details were provided at paragraph 3.6 of the submitted report.

- In terms of how CO2 emissions and noise reduction would be achieved, the applicant said they were committed to meeting the Council's sustainable construction objectives and were looking to achieve CO2 emissions 20% less than the 2013 Part L Building Regulations target, sound insulation was also a high priority.
- Research into the provision for hotel accommodation in this area suggested the demand was high, so at this stage there was no intention to amend the scheme.
- Responding to the question about the sale or retention of buildings by the applicant, the applicant suggested that the residential element is likely to be sold as leasehold with the commercial and retail properties retained.
- In response to whether the scheme would deliver the right balance in terms of office space, it was suggested that there was already a good deal of office space within the area, however, some of the existing office accommodation was located in older buildings some which required modernisation or possible change of use. The applicant considered that there is a demand for new office space and it was considered that the development delivered the right balance.
- On the issue of the City Park it was suggested that the proposal included a design brief as to how the park space should work.
- Members were made aware that the planning permission for the temporary car park would expire in August 2022. It was reported that it was the applicant's intention to reduce the number of car parking spaces over the next five years with a view to offering land for further development. Members noted that included within the Section 106 Agreement for this current application is a contribution towards sustainable travel plan measures.
- Responding to the issue of predicted traffic distribution/ circulation, the Chief Planning Officer had met with Highways England regarding the proposed quantum of development in the South Bank and its impact on the traffic network, bearing in mind work done to date on the Aire Valley Leeds Area Action Plan that had been declared sound by the Planning Inspectorate.
- Responding to the question about café bars/ restaurants and convenience stores, it was considered that the site is part of the City Centre not far from Briggate and the core shopping and office areas and that the flexible ground floor uses would enliven the ground floors of the buildings facing the park and provide activity at lunchtimes, evening and weekends
- Responding to the issue of city centre car parking, the Chief Planning Officer reported that the city had a clear transport strategy, and that discussions had focussed around taking cars out of the City Centre to reduce congestion and pollution, and to improve air quality and place-making in the South Bank. As more development took place and car parking was lost, the Chief Planning Officer suggested that more park and ride provision would most likely feature in this strategy.

In offering comments Members raised the following issues:

- Although one Member indicated he was not supportive of the emerging building heights, overall, Panel Members were supportive of the principle of the application and the proposed design parameters.
- The vision of the scheme was welcomed but there was a desire to see a lot more detail. There were a lot of detailed issues to be addressed at the Reserved Matters stage
- A high quality design for the scheme would be required at Reserved Matters
- Consideration of daylight, sunlight and orientation of residential courtyards would be required at Reserved Matters stage
- Private spaces for residents was important as well as public space
- Members welcomed the proposal to visit a similar scheme in London by the applicant and to have pre-application and associated workshops for the Reserved Matters phases
- It was important that a highway solution was found with Highways England, and if one could not be found the application should be brought back to Plans Panel
- Members welcomed the inclusion of a design brief for the City Park to demonstrate how the park space would work.

In summing up the Chair thanked the Developers for their attendance commenting that this was one of the largest schemes to come before Members in some considerable time. The vision was impressive and Members appeared to be supportive of the application.

## **RESOLVED –**

- (i) That the application be deferred and delegated to the Chief Planning Officer for approval subject to the lifting of Highways England's holding direction regarding the impact of the proposed development on the strategic highway network and the specified conditions set out in Appendix 1 (and any others which he might consider appropriate) and the completion of a Section 106 agreement to include the following obligations:
  - 5% on-site affordable housing in accordance with policy for the area at a prorate mix, split 60:40 lower decile:lower quartile income
  - Off- site highways works being a new pedestrian crossing on Crown Point Road between Bowman Lane and Sheaf Street £70,000, City Connect cycle scheme contribution £364,000 and Sovereign footbridge contribution £500,000
  - Sustainable travel fund £162,488.75
  - Car club contribution £69,350
  - Travel plan monitoring fee £20,000
  - City Park strategy plan
  - Public access through the site including new pedestrian/cycle routes and the City Park

- Cooperation with local jobs and skills initiatives

In the event of the Section 106 not having been completed within 3 months of the resolution to grant planning permission, the final determination of the application shall be delegated to the Chief Planning Officer.

- (ii) That arrangements be made for Members to undertake a visit to a similar scheme by the applicant and that the visit also includes a workshop to provide Members with an opportunity for further understanding of the future detailed Reserved Matters next steps

**60 Application No.17/04351/LA - Position Statement - Construction of a dual carriageway orbital route incorporating new roundabouts, cycle and pedestrian bridges; underpass and overbridge; laying out of country park on land between Ring Road Shadwell and Thorpe Park.**

The Chief Planning Officer submitted a report which sets out a Position Statement in respect of the construction of a dual carriageway orbital route incorporating new roundabouts, cycle and pedestrian bridges; underpass and overbridge; laying out of Country Park on land between Ring Road Shadwell and Thorpe Park.

Site photographs and plans were displayed and referred to throughout the discussion of the application. A 'fly through' of the scheme was also shown to Members.

The Planning Case Officer addressed the Panel, speaking in detail about the proposal and highlighted the following:

- The application is for the East Leeds Orbital Road (ELOR), a two lane dual carriageway 7km in length, connecting the A6120 (Outer Ring Road Shadwell) in the north to Manston Lane in the south where it will connect to the Manston Lane Link Road (MLLR) which is to be delivered as part of the Thorpe Park development. The proposed dual carriageway with segregated pedestrian and cycle route on the southern and western side. The proposed dual carriageway includes five new roundabouts at the A6120, A58, Skeltons Lane, the A64 and Barwick Road. Five new crossing facilities are also proposed:

Red Hall Bridge, Country Park Underpass, Wood Lane/Middle Quadrant Bridge, Southern Quadrant Bridge and Cock Beck Overbridge

- As part of the ELOR proposals, a 9.8ha country park is also to be provided on the land north of the A64 (York Road) and will be to the east of the ELOR. The country park underpass will provide access from the west with the park providing green infrastructure to the road itself and also significant local recreational space for existing and future



residents. The country park is also intrinsic to the drainage proposals for ELOR as a series of SuDS features are proposed.

- The proposed scheme is divided into four main sections:

Red Hall – between the proposed A6120 roundabout and the proposed A58 roundabout

Northern Quadrant – between the proposed A58 roundabout and proposed A64 roundabout

Middle Quadrant – between the A64 roundabout and the new Barwick Roundabout

Southern Quadrant – between the new Barwick Road roundabout and the tie in to the Manston Lane Link Road Red Hall

- In addition to the ELOR there are further transport improvements works proposed at various junctions.
- Explanation was provided that a number of statutory consultees were not fully satisfied with the level of information provided and accordingly further work to resolve these outstanding concerns was underway. Clarification was also provided regarding the assessment provided in paragraph 10.9 of the submitted report to confirm that two separate issues needed to be considered in terms of the Green Belt. The first related to assessing if the proposal had a significant impact on the openness of the Green Belt. The provision of transport infrastructure was considered necessary to enable the delivery of housing and could be considered not to constitute inappropriate development in the Green Belt providing its openness was not adversely affected. Given the scheme did not include significant encroachment into the Green Belt (and where encroachment does occur it would either only have a very limited impact on openness or in the case of the country park represent appropriate development) officers were of the view it did not represent inappropriate development. Regarding the matter of referral, only applications which are considered to represent inappropriate development and that harm is judged to be significant need to be referred to the Secretary of State. With this in mind officers also do not consider at this time there is a need to refer the application to the Secretary of State but this position is being kept under review noting the officer assessment has not yet been formally concluded and all statutory consultees have yet to remove any existing objections.
- As part of the above clarification, question 1 as detailed within the officer report was therefore amended as follows: 1. Do Members accept the principle of the road and country park proposals including the officer's assessment regarding the impact on the Green Belt?

In response to Members questions, the following issues were discussed:

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- Members queried what noise mitigation measures were in place
- Concern was expressed that the Railway Line at Scholes would be severed
- What was the impact on heritage assets
- Had adequate consultation been carried out

In responding to the issues raised, the Planning Case Officer together with the applicant's representative provided the following responses:

- Noise mitigation measures were proposed as part of the scheme including the screening bund which would provide noise attenuation for properties to the west and south. An additional bund will provide noise attenuation for residents of the village of Scholes. Furthermore, 1.8m high noise barriers are proposed along the country park underpass, along the western screening bund and along the western edge of the Cock Beck overbridge. These measures, combined with the intended use of tarmac for the road surface are considered to be sufficient to mitigate the noise impact of the road.
- It was confirmed that as part of the scheme the railway line at Scholes would be severed. The railway line was currently not in use and it would be costly to re-open. There was no intention at this stage that the line would be re-opened.
- The Heritage assets were listed in the submitted report, there was some impact on certain assets: Lazencroft Farm and the Pigeon House at Red Hall, and the new Barnbow designation but Historic England had not raised any objections subject to mitigation measure to ensure their survival.
- On the issue of consultation Members attention was drawn to paragraph 2.16 of the submitted report

In offering comments Members raised the following issues:

- Members supported the principle of a strategic orbital road to add capacity to the road network
- The new road network was a necessary prerequisite if new housing proposals were to proceed.
- Relieving traffic congestion elsewhere on the network would lead to air quality benefits
- The issues raised by the Chair of the Whinmoor Community Forum (Paragraph 6.7 of the submitted report) required further consideration
- Members expressed the view that the design of the Cock Beck Overbridge required improvement in terms of aesthetic quality, the bridge should be transformational, contemporary and functional.
- All crossing points need to be designed as "safe Places" possible use of CCTV for the Country Park underbridge
- Some of the footpath/cycle connections required to be looked at in more detail to ensure they were as direct as they could be

In drawing the discussion to a conclusion Members provided the following feedback;

- Members accepted the principle of the road and country park proposals including the officer assessment of the impact on the Green Belt
- Members accepted the design approach for ELOR (e.g. 50 MPH, limited junctions) in terms of addressing the strategic objectives for facilitating housing growth/ traffic relief in this part of the city subject to treatment of the existing Green Roads.
- Members accepted the proposed crossing measures to address connectivity issues for none motorised users, subject to further consideration that all crossing points were designed as “safe places” possible use of CCTV
- That subject to further consideration on the design of one of the bridges, Members were of the view that the visual impact of the ELOR and the impact on heritage assets was acceptable.

In summing up the Chair said Members were supportive of the proposal and welcomed the progression of the application

#### **RESOLVED –**

- (i) To note the details contained in the Position Statement
- (ii) That the developers be thanked for their attendance and contribution.

#### **61 Application No.17/03974/RM - Position Statement - Reserved Matters Application for 292 dwellings including layout, scale, appearance, landscape and access. Northern development pots on land south of railway line at Thorpe Park, Leeds, LS15 8ZB**

The Chief Planning Officer submitted a report which sets out a Position Statement in respect of a Reserved Matters application for 292 dwellings including layout, scale, appearance, landscape and access. Northern development pots, on land south of railway line at Thorpe Park, Leeds, LS15 8ZB.

Site photographs and plans were displayed and referred to throughout the discussion of the application.

The Planning Case Officer addressed the Panel, speaking in detail about the proposal and highlighted the following:

- The application relates to the northern half of the employment allocation at Thorpe Park that totalled approximately 65 hectares.
- The site is located to the south of the Leeds-York railway line and Manston Lane, west of the M1 (junction 46), north of the A63 Selby Road and the existing Thorpe Park buildings.

- Austhorpe Lane is to the west. The site covers Zone B of the Thorpe Park masterplan but excludes what it termed 'Central Park', which is an important landscaping and open space feature running east to west across the site and which contains the SUDS attenuation/balancing ponds.
- The application seeks approval of the detailed reserved matters relating to layout, scale, appearance, landscaping and detailed access arrangements for dwellings [the main access from a signalised junction from the north south Manston Lane Link road is approved].

In response to Members questions, the following issues were discussed:

- Some Members welcomed the modern/ contemporary design house types but queried why character areas had been created offering traditional house types. Could the modern/ contemporary design look be not used throughout the whole of the development.

In responding to the issues raised, the applicant's representative provided the following response:

- The applicants took the view that commercially, mixed house types were popular and appealed to the market. It was also suggested that the transition from contemporary to traditional offered diversity.

In offering comments Members raised the following issues:

- A minority of Members expressed the view that it was better to have the same design throughout
- Members welcomed the demonstration of in-roof-plane solar photovoltaics
- Members requested colour CGI images of the more traditional house types to be provided together with those of the more contemporary house types when the application comes back before Members.

In drawing the discussion to a conclusion Members provided the following feedback;

- Members accepted the approach of having two character areas and the more contemporary design and suggested that the transition from one area to another should flow/mix
- Member were content with the active frontages with Central Park
- Members were supportive of the overall masterplan in relation to the green infrastructure, specifically the easternmost green finger
- Members were supportive of the scheme, commenting that some real positive changes had been made following Members feedback at the pre application stage.

In summing up the Chair said Members were supportive of the proposal and welcomed the submission of the Reserved Matters application

**RESOLVED –**

- (i) To note the details contained in the Position Statement
- (ii) That the developers be thanked for their attendance and contribution.

**62 PREAPP/17/00604 - Pre- Application Presentation for a 6 storey apart-hotel with ground floor commercial uses and accesses to Leeds City Market at 4-32 George Street, Leeds 2.**

(Having already declared a personal interest in this item, Councillors: J McKenna and Macniven withdrew from the meeting – Councillor McKenna vacated the Chair, Councillor P Gruen assumed the Chair)

The Chief Planning Officer submitted a report which sets out details of a Pre-application proposal for the for a 6 storey apart-hotel with ground floor commercial uses and accesses to Leeds City Market, 4-32 George Street, Leeds 2.

Site photographs and plans were displayed and referred to throughout the discussion of the application.

The applicant's representatives addressed the Panel, speaking in detail about the proposal and highlighted the following:

- The site is located on the southern side of George Street, backing on to the former Butchers Row and the adjoining modern market hall.
- The site is currently occupied by 1930's single storey rendered, flat roofed, commercial units on its western half and 1980's two storey shops and office buildings to the east. Between these sits an entrance to the market building.
- To the west the site abuts the Grade I listed Leeming House and to the east is a further entrance to the modern market hall and an electricity sub-station and toilet block.
- To the north, on the opposite of George Street, is the new Victoria Gate development. The western end of the site is within the City Centre Conservation Area.
- The proposal is for a single building which fronts the back edge of footpath.
- The upper floors of the building are proposed to be used as an apart-hotel which would be accessed via at a point in the centre of the

façade. This would contain approximately 117 separate units of accommodation with 82 studios, 31 one bedroom and 4 two bedroom units. The ground floor is to contain 9 units for a range of commercial uses - retailing (A1), café/restaurant (A3), bar (A4) and takeaway (A5).

- The elevations are primarily of brick, however, given the total length is approximately 120m the building is punctuated approximately halfway along by a recessed 'slot' which is clad in glass. This is the location of the replacement entrance to the northern side of the market hall and contains a double storey height void which is designed to mirror the existing Leeming House market entrances. In addition the brick part of the building terminates 5m from Leeming House where a 2 storey glass addition makes the transition between the old and the new. This allows the stone gable of Leeming house to remain visible as well as retaining a view through to the dome of the market roof beyond.
- The elevations to George Street divide the building into a definite base, middle, top with the base consisting of a double height arch to reference the height of the base of Leeming House, the middle being 3 storeys of brick with paired windows set into shallow reveals and decorative brickwork pattern, and the top being a metal clad mansard with feature dormer windows, again paired to relate to the elevations below. In addition the building steps down the hill, emphasised by the ground floor unit configuration and the eaves line, which is stepped once within each wing. The southern elevation, which faces out over the market roof, is of a much simpler design but is also in brick with regular window pattern, standing seam zinc slots and louvre detail.
- A number of the ground floor commercial units would have the ability to be accessed from both George Street and the former Butchers Row, which offers the opportunity to access the market from multiple points along George St. This also retains the commercial unit frontages along the northern side of Butcher's Row, a number of which would be able to be accessible at-grade, an improvement over the current situation. The requirement to provide a fire exit at one point along Butchers Row results in the inability to provide a commercial frontage. At this point the elevation has a dummy retail frontage which can be used to advertise market events & produce and house art installations. The apart-hotel also has a frontage directly into the market from this elevation which will help to drive increased footfall and provide further life and activity.
- The upper floors incorporate both plant areas and vertical risers within the envelope of the building which means that there are no requirements for flues or air conditioning units to be located on the outside of the building. Refuse storage area are also included within the ground floor footprint and accessed from discreetly located doorways.
- At the eastern end of the site cycle storage is proposed to be contained behind the commercial frontages and so would not impact significantly

on the elevational treatment. The market entrance is marked by a high-level 'Leeds Market' signage arch. This is designed to swing open to allow vehicles to access the area in order to service the adjacent electricity sub-station. Servicing and drop-offs would all occur from George St which was reconfigured as part of the recent Victoria Gate scheme. This proposal would also bring forward the final treatment of the footway, as a temporary blacktop treatment was laid in advance of the necessary construction works.

In response to Members questions, the following were raised:

- The proposals for the George Street elevation were welcomed but concern was expressed about the proposals for Butchers Row.
- There was some concern about the use of glass as the entrance material
- What was the rationale for an Aparthotel in this location
- Would the Market Committee Plaque be relocated
- Would any assistance be provided to ensure the existing businesses were relocated.

In responding to the issues raised, the applicant's representatives said:

- The proposals for Butchers Row would be sympathetic and would retain many of the traditional elements
- The use of glass was chosen to break up the brick mass, the design also marks the entrance to the Market.
- The proposal for an Aparthotel was chosen because there was a demand for customers to stay longer in this area.
- The applicant confirmed that the Market Committee plaque would be relocated within the new development.
- There was a desire for enhancement of the Market and it would be for the Kirkgate Market Board to consider if existing businesses would be assisted in relocating.

In offering comments Members raised the following issues:

- The majority of Members supported the use of glass for the entrance however, one Member was of the view that the use of glass did not enhance the building.
- Could the windows be recessed and could more detailing around the windows be incorporated.
- There was some concern that the new façade may hide the Market building.

In drawing the discussion to a conclusion Members provided the following feedback;

- Members considered the proposed uses were acceptable

- That with the inclusion of the suggested amendments, the design for the George Street elevation was acceptable.
- The design for the elevation to Butchers Row was acceptable.

In summing up the Chair said Members were supportive of the proposal and welcomed the submission of a formal application

**RESOLVED –**

- (i) To note the details contained in the pre-application presentation
- (ii) That the developers be thanked for their attendance and presentation

**63 Date and Time of Next Meeting**

**RESOLVED –** To note that the next meeting will take place on Thursday 2<sup>nd</sup> November 2017 at 1.30pm in the Civic Hall, Leeds.





Originator: C. Briggs

Tel: 0113 2224409

## Report of the Chief Planning Officer

### *City Plans Panel*

Date: 2<sup>nd</sup> November 2017

**Subject: Planning application reference 17/02666/FU for a ten storey block of 101 apartments with ground floor residents lounge, cinema room, gym and cycle storage room at 16-18 Manor Road, Holbeck, Leeds LS11 9AH**

Applicant	Date Valid	Target Date
Citylife Developments	24.05.2017	17.11.2017

#### Electoral Wards Affected:

City and Hunslet

Yes Ward Members consulted

#### Specific Implications For:

Equality and Diversity

Community Cohesion

Narrowing the Gap

**RECOMMENDATION: DEFER and DELEGATE to the Chief Planning Officer for approval subject to the satisfactory resolution of potential wind impact, and subject to the specified conditions set out in Appendix 1 (and any others which he might consider appropriate) and the completion of a Section 106 agreement to include the following obligations:**

- 5% Affordable housing to be provided in accordance with details at section 10.8 of this report
- Sustainable travel fund £14,803
- Car club contribution £10,000
- Amendments to traffic regulation order to replace 3 pay and display bays to accommodate loading and car club £7,500
- Revenue compensation for loss of 3 pay and display bays £18,000
- Travel plan monitoring fee £2505
- Public access through the site
- Cooperation with local jobs and skills initiatives
- Management fee £3000

## 1.0 Introduction

1.1 This report relates to a full planning application for a ten storey block of 101 apartments with ground floor residents lounge, cinema room, gym and cycle storage room on vacant brownfield land in Holbeck, South Bank in the City Centre.

## **2.0 Proposal**

2.1 The proposal is for 101 flats, made up of 21 studio flats, 56 one-bed flats, 20 two-bed flats and 4 three-bed flats. These would be made available to the Private Rented Sector (PRS). No car parking is proposed as part of the scheme. The building would be 10 storeys, including the top 2 storeys set back to create a roof terrace. The architectural concept for the building is a folded sculptural metal form, finished in bronze cladding, with balconies and screens created by the folds in the façade. The former use of the site was a metal fabrication business.

2.2 In terms of dwelling size, the following flat sizes would be provided:

- Studio flat 31sqm
- One-bed flat 39sqm or 41sqm
- Two-bed flat 63sqm
- Three-bed flat 74sqm

2.3 At ground floor, the entrance to the flats would be on the Manor Road frontage at the western end of the building. The proposal would also include a new pedestrian/cycle link between Manor Road and Siddall Street under an oversailing part of the building. This would create a direct link between David Street and Sweet Street. The route would be activated by a resident's gym, and would be lit and covered by CCTV. The majority of the route would be overlooked by flats at Manor Mills. The proposal would provide additional natural surveillance from the building entrance close to the corner of the building, and from upper floors over the existing indirect pedestrian route that runs to the west of the site.

2.4 The application proposal is supported by the following submitted documents:

- Planning Statement
- Statement of Community Involvement
- Affordable Housing Statement
- Affordable Housing Pro Forma
- CIL / Section 106 Heads of Terms
- Completed CIL Additional Questions Form and Form 2 (social housing relief)
- Design & Access Statement incorporating Sustainability Statement
- Full scaled plans, elevations and sections
- Planning application form
- Noise Assessment
- Transport Statement
- Wind Study
- Travel Plan
- Flood Risk and Drainage Assessment
- Flood Risk Sequential Assessment Test
- Phase 1 Ground Report

## **3.0 Site and Surroundings**

3.1 The site is currently an unsightly, longstanding cleared vacant brownfield site (0.1 hectare) in the Holbeck Urban Village area of Leeds South Bank, in the designated City Centre. The site is on the south side of Manor Road at the junction with David Street. The immediate surrounding context includes Manor Mills an 8-9 storey residential development to the east, a single storey telecommunications centre (with permission for office use) to the west, and a single storey warehouse to the south. The wider context includes the Dandara residential development, approved at Plans Panel in 2015, and now under construction, and the 8 storey offices at The Mint on Sweet Street. Opposite the site to the east of David Street are 3 storey beige brick offices, and to the west are the single storey industrial units at Leodis Court. The

Grade I listed Temple Works, a former flax spinning mill in the Egyptian revival style, lies at the western end of Manor Road at the junction with Marshall Street, which is also the eastern edge of the Holbeck Conservation Area. The immediate surrounding area offers a wide variety of architectural styles and materials including red brick, light and dark brown brick, render, glazed balconies, copper cladding, and natural stone.

3.2 There is an existing pedestrian route to the west of the site, this is in the ownership of the adjoining landowner. This is not public highway, but it is a claimed Public Right of Way.

3.3 The site lies in flood risk zone 2.

#### **4.0 Relevant Planning History**

4.1 Planning reference 20/537/04/FU for an 8 storey block comprising 45 flats, first floor office and ground floor A1/A3 retail/cafe unit and car parking was approved in 2005. This was subsequently amended by planning reference 20/545/05/FU, in order to add 12 additional flats. This second planning permission was approved in 2006, and expired in 2011.

#### **5.0 History of Negotiations**

5.1 Pre-application discussions on the current proposals commenced in November 2016. One meeting was held between the agent and architect with planning, highways and design officers. City and Hunslet Ward Councillors were consulted on 8<sup>th</sup> March 2017. The applicant states that they undertook a pre-application consultation with local residents by posting notices in the reception of Manor Mills and with other local businesses.

5.2 The architect made a pre-application presentation to City Plans Panel on 6<sup>th</sup> April 2017 and the following issues were discussed:

- The angle of the walls would prevent overlooking of other properties.
- The meshed metal shutter would feature on all sides of the building.
- The height of the link through the building was felt to be sufficient and would allow the flow of natural light.
- Affordable housing – whilst the applicant would like to commit to on-site provision of affordable housing, experience on similar projects had shown this to be difficult due to the low number of units available and the difficulty in engaging a social landlord to manage these. It was considered that a commuted sum may be the most appropriate route. It was further mentioned that there could be other options through buy to rent or sub market discounted housing. Members expressed concern regarding the lack of on-site affordable housing.
- Footpaths around the site and relation to adjacent sites which would be subject of future development.
- The cladding would be a copper coloured aluminium and with correct maintenance would retain its quality of finish. There would not be an issue with reflections from car headlights as the lower part of the building would not have the cladding finish.

Members commented as follows:

- Members supported the size and quality of residential dwellings proposed.
- Members agreed that the proposed building layout, height and emerging design was generally acceptable.
- With regard to the lack of on-site parking there was some concern that this could cause problems to neighbouring areas. Questions were asked whether

basement parking could be provided. It was stated that basement parking would not be feasible due to space. It was considered that due to the location of the development that a no car scheme was potentially suitable. Further consideration to this matter would be given as part of the Transport Assessment at the next stage of the application.

## **6.0 Public/Local response**

### **6.1 Planning Application publicity**

- Site Notice 09.06.2017
- Press Notice 09.06.2017
- City and Hunslet Ward Councillors consulted 31.05.2017

### **6.2 1 comment from Leeds Civic Trust (LCT)**

- LCT support the proposed use and the design of the building but object to its height. The design and cladding of the building will give it a distinctive appearance, differentiating it from the rather indifferent designs of recent similar buildings around it. Its smaller footprint than most other developments of this type also provides some variety. However, the Holbeck SPD, proposes that buildings should be reduced in height the closer they are to the Grade I listed Temple Mill, guidance which the Trust supports. Logically the proposed building should therefore be lower than the adjacent Manor Mills building, but is in fact two storeys, 5m, higher. The fact that the cladding height is the same as Manor Mills and the additional storeys are set back slightly does not mitigate this anomaly, where it is building outline that is important, not façade alignments.
- LCT are particularly supportive of the proposal to open up a public pedestrian route through the building.

### **6.3 3 objections have been received from residents at Manor Mills, expressing the following concerns:**

- Impact on daylight/sunlight and visual dominance. It is not clear how the proposal will impact on the views of the existing dwellings at the northern internal corner of Manor Mills and the Design and Access Statement fails to provide a visual of this relationship. The proposed development is greater in height and is as such overbearing in nature. There will be a very significant loss of natural daylight and views for apartments facing the proposed development.
- The proximity of the proposed building is too close to Manor Mills, creating a major privacy concern. A distance of 28 metres cannot possibly be considered a large separation.
- Noise pollution from the proposed building will be intrusive, disrupting sleep for residents during the build and into the future.
- The gaudy bronze cladding is not aesthetically pleasing, rather appearing rusted. Further, the glare from the metal surface is liable to cause problems for motorists, pedestrians and Manor Mill residents alike.
- The building is at odds with the architecture of the area, it seems no attempt has been made to make it fit in with its surroundings.
- The visuals show the proposed building linked to the side of Manor Mills: is that intentional, or a design error
- An increase in traffic to the area is a concern.
- There is likely to be an increase in litter issues - Leeds Council already struggles to keep the area tidy.
- The land is better suited for parking spaces, a garden, or a single storey building.
- Concern about lack of pre-application consultation with leaseholders
- Impact on the setting of the Grade I listed Temple Works due to height and lack of a heritage assessment in the planning application

- Private rented sector housing will create a transient population which could damage the long term sustainability of Holbeck Urban Village.

## **7.0 Consultations**

### **7.1 Statutory Consultations**

#### 7.1.1 Historic England

At the time of writing this report, Historic England comments are awaited. Any comments received will be updated verbally at Panel.

#### 7.1.2 Coal Authority

The application site does not fall within the defined Development High Risk Area. There is no requirement in this case to consult the Coal Authority.

#### 7.1.3 Environment Agency

No objection subject to passing the flood risk sequential test, and conditions regarding the implementation of the measures set out in the submitted flood risk assessment

### **7.2 Non-statutory Consultations**

#### 7.2.1 Leeds City Council (LCC) Transport Development Services

Highways officers have advised that the proposal is acceptable in principle, subject to the provision of identified off-site highways works including informal pedestrian crossing provision and traffic calming measures at the junction of David Street with Manor Road, improvements to the north end of Siddall Street, provision of a loading bay on Manor Road and provision of a car club bay, reinstatement of footway and full height kerbs at redundant vehicle accesses and associated new/amended TRO's the inclusion of the Travel Plan in the Section 106 agreement, a sustainable travel fund, and secure cycle parking.

#### 7.2.2 LCC Flood Risk Management

No objection subject to a condition that requires the surface water greenfield discharge rate to be applied (5l/s per hectare) unless it can be justified it is not feasible.

#### 7.2.3 LCC Public Rights of Way

A claimed footpath runs from Manor Road to Siddall Street [along the western and southern edge of the site outside the application boundary]. This footpath is well used and looks to have been laid out for the public to use with a tarmacked and flagged surface. The surface is rough and in a general state of disrepair. As the development is likely to cause an increase in use by the public the footpath should be resurfaced for the betterment of the development and the wider community. The footpath should have formal status by adoption as a highway or recorded as public right of way by Creation Agreement, this will avoid problems over appropriate use and responsibility for maintenance in the future.

#### 7.2.4 West Yorkshire Combined Authority (WYCA)

To encourage the use of sustainable transport as a realistic alternative to the car, the developer needs to fund a package of sustainable travel measures. WYCA recommend that the developer contributes towards sustainable travel incentives to encourage the use of sustainable modes of transport. Leeds Council have recently introduced a sustainable travel fund. The fund can be used to purchase a range of sustainable travel measures including discounted MetroCards (Residential MetroCard Scheme) for all or part of the site. This model could be used at this site. The payment schedule, mechanism and administration of the fund would have to be

agreed with Leeds Council and WYCA and detailed in a planning condition or S106 agreement. As an indication of the cost should the normal RMC scheme be applied based on a bus only ticket, the contribution appropriate for this development would be £49,606.15. This equates to Bus Only Residential MCards.

- 7.2.5 West Yorkshire Police  
Advice given regarding security, access control, cycle parking and CCTV in the interests of the safety of future residents.
- 7.2.6 LCC Nature Conservation  
There should be no significant nature conservation concerns provided the site is not cleared between 1<sup>st</sup> March and 31<sup>st</sup> August unless a competent ecologist has checked for active birds nests within 24 hours of workings commencing, in order to protect nesting birds in accordance with the Wildlife and Countryside Act 1981 (as amended).
- 7.2.7 LCC Environmental Protection  
No objection subject to the implementation of the sound insulation measures detailed in the submitted noise report.
- 7.2.8 LCC Conservation Team  
No objection.

## **8.0 Planning Policy**

### **8.1 Statutory Context**

- 8.1.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires the application to be determined in accordance with the development plan unless material considerations indicate otherwise. For the purposes of decision making, the Development Plan for Leeds currently comprises the following documents:
- The Leeds Core Strategy (Adopted November 2014)
  - Saved UDP Policies (2006), included as Appendix 1 of the Core Strategy
  - The Natural Resources & Waste Local Plan (NRWLP, Adopted January 2013) including revised policies Minerals 13 and 14 (Adopted September 2015).

### **Development Plan**

#### **8.2.1 Leeds Core Strategy 2014**

The Core Strategy sets out strategic level policies and vision to guide the delivery of development investment decisions and the overall future of the district. Relevant Core Strategy Policies include:

- Spatial policy 1 location of development
- Spatial policy 3 role of Leeds City Centre
- Spatial policy 6 housing requirement and allocation of housing land
- Spatial policy 8 economic development priorities
- Policy CC1 City Centre development
- Policy CC2 City Centre south
- Policy CC3 improving connectivity between the City Centre and neighbouring communities
- Policy H3 density of residential development
- Policy H4 housing mix
- Policy H5 affordable housing
- Policy EN1 carbon dioxide reduction
- Policy EN2 sustainable design and construction

- Policy EN4 district heating
- Policy EN5 managing flood risk
- Policy G9 biodiversity improvements
- Policy P10 design
- Policy P11 heritage
- Policy P12 landscape
- Policy T1 transport management
- Policy T2 accessibility requirements and new development

### 8.2.2 **Leeds Unitary Development Plan Review 2006 Saved Policies**

Relevant saved policies include:

- Policy GP5 all relevant planning considerations
- Policy BD2 new buildings
- Policy BD5 residential amenity
- Policy LD1 landscaping
- CC31A Holbeck Urban Village Strategic Housing and Mixed Use Site

### 8.2.3 **Leeds Natural Resources & Waste Plan 2013**

The Natural Resources and Waste Development Plan Document (Local Plan) is part of the Local Development Framework. The plan sets out where land is needed to enable the City to manage resources, like minerals, energy, waste and water over the next 15 years, and identifies specific actions which will help use natural resources in a more efficient way. Relevant policies include:

- Minerals 2 minerals safeguarding area sand and gravel
- Minerals 3 mineral safeguarding area coal
- Air 1 management of air quality through new development
- Water 1 water efficiency
- Water 2 protection of water quality
- Water 4 development in flood risk areas
- Water 6 flood risk assessments
- Water 7 surface water run-off
- Land 1 contaminated land

### 8.3 **Relevant Supplementary Planning Guidance includes:**

Leeds Parking SPD

Street Design Guide SPD

Neighbourhoods for Living SPG

Travel Plans SPD

Accessible Leeds SPD

Building for Tomorrow Today: Sustainable Design and Construction SPD

Holbeck South Bank SPD 2016

### 8.4 **Holbeck South Bank SPD 2016**

The Holbeck South Bank SPD was adopted in June 2016 as an updated revision to the 1999 and 2006 Holbeck Urban Village Planning Frameworks. As with earlier versions the main aim of the SPD is to create vibrant, sustainable, mixed use communities whilst safeguarding the unique historic character of the area. The whole of Holbeck, South Bank, is designated as a mixed-use area which should include a mixture of working, living, retailing and recreational opportunities. It is intended that the area should meet some of the identified need for City Centre housing. Further improvements to connectivity are encouraged. A pedestrian and cycling friendly environment is sought in part by minimising through traffic in the area. Encouragement will also be given to developing with the minimum acceptable parking provision. The site falls within the Eastern Gateway character area.

It may be appropriate for new buildings to gradually increase in height away from the

listed buildings. Buildings should define street frontages and provide pavement widths responding to building scale. New footpath links are encouraged and new public realm should be given an adequate sense of enclosure by the buildings that define them. Relevant specific proposals for this area include the extension and enhancement of the footpath linking Manor Road to Siddall Street as part of a new network of routes between Manor Road, Sweet Street and Marshall Street.

## 8.5 **National Planning Policy Framework (NPPF)**

The NPPF identifies 12 core planning principles (para 17) which include that planning should:

- Proactively drive and support sustainable economic development to deliver homes;
- Seek high quality design and a good standard of amenity for existing and future occupants; and
- Actively manage patterns of growth to make the fullest possible use of public transport, walking and cycling.

Paragraph 49 states that housing applications should be considered in the context of the presumption in favour of sustainable development. The NPPF states that local authorities should deliver a wide choice of homes, widen opportunities for home ownership and create sustainable, inclusive and mixed communities (para 50).

Section 7 states that good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people. It is important that design is inclusive and of high quality. Key principles include:

- Establishing a strong sense of place, using streetscapes and buildings to create attractive and comfortable places to live, work and visit;
- Optimising the potential of the site to accommodate development;
- Respond to local character and history;
- Reflect the identity of local surroundings and materials, while not preventing or discouraging appropriate innovation;
- Create safe and accessible environments; and
- Development to be visually attractive as a result of good architecture and appropriate landscaping.

Paragraph 61 states that although visual appearance and the architecture of individual buildings are very important factors, securing high quality and inclusive design goes beyond aesthetic considerations. Therefore, planning policies and decisions should address the connections between people and places and the integration of new development into the natural, built and historic environment.

Paragraph 131 states that in determining planning applications local planning authorities should take account of the desirability of sustaining and enhancing the significance of heritage assets, the positive contribution that conservation of heritage assets can make to sustainable communities including their economic viability, and the desirability of new development making a positive contribution to local character and distinctiveness.

Paragraph 132 of the NPPF states that when considering the impact of a proposed development on the significance of a designated heritage asset great weight should be given to the assets conservation, and that the more significant the asset the greater the weight should be. It also states that significance can be harmed through development within its setting, and that substantial harm to or loss of designated



heritage assets of the highest significance (Grade I and II\* listed buildings and Scheduled Ancient Monuments) should be wholly exceptional.

Paragraph 134 states that where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal, including securing its optimum viable use.

Paragraph 137 states that Local planning authorities should look for opportunities for new development within Conservation Areas and World Heritage Sites and within the setting of heritage assets to enhance or better reveal their significance. Proposals that preserve those elements of the setting that make a positive contribution to or better reveal the significance of the asset should be treated favourably.

## **8.6 National Planning Practice Guidance (NPPG)**

This provides Central Government Guidance on a range of planning matters and provides the following advice:

### **“The private rented sector**

Some privately rented homes can come from purpose built schemes held in single ownership which are intended for long term rental. The economics of such schemes differ from build to sale and should be determined on a case by case basis. To help ensure these schemes remain viable while improving the diversity of housing to meet local needs, local planning authorities should consider the appropriate level of planning obligations, including for affordable housing, and when these payments are required. So these homes remain available to rent only, Local Planning Authorities may choose to explore using planning obligations to secure these schemes for a minimum period of time. Local Planning Authorities should enforce these planning obligations in the usual way.”

## **8.7 Other material considerations**

### **8.7.1 Emerging Site Allocations Plan (SAP)**

The site is allocated for housing in the emerging SAP for 57 flats under site reference HG1-463.

### **8.7.2 Draft Holbeck Neighbourhood Plan**

Although the draft Plan area is to the south of Sweet Street and does not directly affect this site, a key objective of the plan is to seek better connections for pedestrians, cyclists and public transport in the area between Holbeck and the City Centre. The draft Neighbourhood Plan was subject to public consultation in 2016, however it has little planning weight at this stage.

### **8.7.3 The Leeds Standard and the DCLG Technical Housing Standards**

The Leeds Standard sets out the importance of excellent quality housing in supporting the economic growth ambitions of the Council. The Leeds Standard sizes closely reflects the Government’s Technical Housing Standards – Nationally Described Space Standard which seek to promote a good standard of internal amenity for all housing types and tenures. Whilst neither of these documents has been adopted as formal planning policy in Leeds given their evidence base in determining the minimum space requirements they are currently used to inform decisions on the acceptability of development proposals. The Council has committed to prepare a Development Plan Document (DPD) which will allow the national standards to be applied to new housing development in Leeds. This is

programmed to be incorporated within the Core Strategy selective review, with public consultation taking place later this year.

- 8.7.4 Private Rented Sector (PRS) / Build To Rent (BTR) and Affordable Housing  
On 22 March 2017 Leeds City Council's Executive Board endorsed an approach which recognises that the acceptance of commuted sums from PRS/BTR schemes may be appropriate and justified in accordance with Core Strategy Policy H5.

## **9.0 Main Issues**

- 9.1 Principle of the proposed use
- 9.2 Residential quality and sustainability
- 9.3 Design, scale and layout
- 9.4 Highways and transportation
- 9.5 Flood risk and sustainable drainage
- 9.6 Wind
- 9.7 Other considerations
- 9.8 Planning obligations and Community Infrastructure Levy (CIL)

## **10.0 Appraisal**

### **10.1 Principle of the proposed use**

- 10.1.1 The redevelopment of the brownfield site in a highly sustainable City Centre location would meet the Core Strategy objectives including policies SP3, CC1, H2, CC2, and CC3, by regenerating a vacant brownfield site, delivering new homes, and new pedestrian connections. It would also meet the aims of the existing saved site allocation (CC31A) and the emerging Site Allocations Plan (HG1-463). The proposal would contribute to meeting the adopted Holbeck South Bank SPD vision for the area, to create a mixed-use City Centre neighbourhood, including a mixture of working, living, retailing and recreational opportunities.

### **10.2 Residential Quality and Sustainability**

- 10.2.1 The majority of the accommodation would meet the Nationally Described Housing Standards, with the exception of the studio flats which would be 31sqm. These are similar in size to those approved and under construction at the nearby Dandara site to the east along Manor Road. However, it is considered that the proposed layout, daylight, circulation and juxtaposition of living functions in these flats would be satisfactory and on balance it is considered that these are acceptable.
- 10.2.2 Core Strategy Policy H4 on Housing Mix requires a minimum provision of 20% of flats be provided containing 3 bedrooms across the Local Authority area. However, the policy allows the nature and location of the scheme to be taken in to account when assessing housing mix. Given that this scheme offers approximately 4% three-bed flats, and the provision of three-bed dwellings in the city centre currently stands at around 1%, this proposal would exceed the percentage of current provision in the area. Generally a 5% provision of three bed flats has been achieved in most major city centre residential planning applications approved since the adoption of the Core Strategy. On balance it is considered that the proposed mix would offer a sufficient variety in unit type for a scheme of this size in this City Centre location.
- 10.2.3 The application is supported by a Sustainability Statement, which sets out how 10% on-site low carbon energy generation would be achieved on site with roof-top solar panels, and confirms that 20% betterment on carbon dioxide emissions above the 2013 Part L building regulations, and the water usage target, would be achieved on-

site. These measures are required by Core Strategy Policies EN1 and EN2, and would be controlled by planning condition.

### 10.3 **Design, scale and layout**

- 10.3.1 The Holbeck South Bank SPD 2016 (Part 2, Section 11 Eastern Gateway area p42-3) states that there is potential for a building at this site to be 7-9 storeys in height. Whilst this is a guideline, there is a generally consistent emerging height and building line to this part of Manor Road, formed by City Walk and Velocity in the east, the Dandara scheme that is under construction (14/04641/FU), and the Manor Mills flats adjacent to this site. The remainder of Manor Road to the north and west is low rise. It is considered that development along Manor Road should generally step down towards the Grade I listed Temple Mill, which is framed at the western end of the street. Marshall Street is also the boundary of the Holbeck Conservation Area, which lies to the north and east of the site. The increase in height by one/two storeys to ten storeys (approximately 30m) adjacent to Manor Mills, which is part eight/part nine storeys, would act as a focal point at the junction of David Street, and due to the setting back of the top two storeys, is not considered to be a sufficient increase in scale to be out of character with the emerging streetscene along Manor Road from the Dandara flats, Manor Mills and the application proposal. There is also sufficient distance, approximately 100m, to the listed Temple Mill to avoid an overbearing and overdominant effect on its setting. Section 66(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990 states that Local Planning Authorities (LPAs) must have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses and pay special attention to the desirability of preserving or enhancing the character and appearance of a Conservation Area. There is a variety of heights and scale in the area, and the general built form along this part of the block between Manor Road and Sweet Street is around 30 metres in height. Buildings generally step down in scale to Temple Mill and the western end of Manor Road. It is not considered that the proposal, would obscure or interrupt views of the principal interest facades of Temple Mill from Manor Road, would not adversely affect the special character or significance of the setting of the Grade I listed building. It is considered that the regeneration of the site with a high quality well-designed building would enhance the setting and significance of Temple Mill, and the character and appearance of the nearby Holbeck Conservation Area, in accordance with Core Strategy Policies P10 and P11.
- 10.3.2 It is considered that the proposed angled bronze cladding treatment is positive and imaginative, and would in principle be supported. The architectural treatment would provide visual interest to Manor Road, by contrasting with the recent developments, and enhancing the character of the area, and the setting of Temple Mill in longer views along the street. In response to objector comments about the risk of glare from the sun, the architect has confirmed that the bronze cladding panel would have a matt finish, which would minimise this risk. Exact details and large-scale samples of cladding materials would be controlled by planning condition.
- 10.3.3 The Holbeck South Bank SPD promotes a series of new pedestrian routes and spaces, alongside surrounding buildings and infrastructure. The provision of a new more direct pedestrian route through the building, at a width of approximately 6m, would be additional to the existing pedestrian route adjoining the site. The new route would promote better links between Water Lane, David Street and Sweet Street, as a contribution to a wider set of new connections encouraged by the SPD.
- 10.3.4 Three local residents have raised concern regarding the impact of the proposal on potential for overlooking and loss of privacy, visual dominance, outlook,

overshadowing and loss of light to flats at Manor Mills. The building would be constructed next to the blank western gable of Manor Mills, and would then step in by some 6m away from the shared boundary. The distance between the west-facing elevation with windows of Manor Mills and the eastern flank of the proposal would be approximately 28m, which is considered to be acceptable in a City Centre environment, between buildings of a similar scale, in terms of privacy, outlook, daylight and sunlight. Whilst the building would have a visual impact on the outlook of south facing flats on the northern wing of Manor Mills, it is considered that there would be sufficient separation across the new public route between the two buildings at this point. Given the orientation of the south facing flats at Manor Mills, whilst there may be some loss of sunlight it is considered that there would be sufficient daylight and that there would not be a significant adverse impact on residential amenity in this case. It is considered that there would be satisfactory privacy relationships between the proposed building and its neighbours in terms of distances between facing habitable room windows across and around the scheme. Within the context of a dense edge of centre urban grain it is considered that the distances between habitable room windows are generally acceptable and allow sufficient clear outlook within the site boundary. The south facing north wing of Manor Mills would be over 6m away from the proposal as it runs southwards, however the shutter design on the windows of the application proposal would limit overlooking between the two buildings. It is considered on balance in this context that the proposal would meet Core Strategy Policies P10 and CC1 and Saved UDPR Policies GP5 and BD5.

#### 10.4 **Highways and transportation**

10.4.1 The Holbeck South Bank SPD highlights the desire to produce a pedestrian and cycling friendly environment, in part by minimising through traffic in the area and by developing with the minimum acceptable parking provision. The site is located in a sustainable City Centre location within a 10 minute walking distance of employment, retail, leisure, supporting facilities and public transport including Leeds Station Southern Entrance and bus stops along Meadow Lane. The scheme would therefore meet the Policy T2 and the Accessibility Standards in Table 2 of Appendix 3 of the Core Strategy. The site is also within a controlled and enforced parking area, with pay and display spaces on the street in the vicinity of the site. The scheme proposes no car parking spaces. The scheme would be supported by an acceptable travel plan which would be attached to the Section 106 legal agreement. Also included in the legal agreement is the provision of a Sustainable Travel Fund for the site, which would include car club trial provision for residents. In relation to WYCA comments, Travel Plan measures have been agreed with the applicant, and a Sustainable Travel Fund sum has been calculated based on the latest Travel Plan guidance. The Sustainable Travel Fund sum is based on half the equivalent cost of residential metrocards in a City Centre location, to take account of the increased likelihood that City Centre residents would walk or cycle to work. Part of the sum is also identified for City Car Club trial provision, in this case £10,000. The Travel Plan Coordinator has the option of purchasing metro cards with the remaining £14,803 Sustainable Travel Fund sum if these are appropriate to the residents at the time. Secure long stay cycle parking would also be provided on-site. On this basis, it is considered that a zero car parking provision would be acceptable in this sustainable City Centre location, where on-street parking is managed and enforced.

10.4.2 Highways officers have confirmed that the scheme is acceptable in terms of arrangements for deliveries and refuse/recycling servicing. In order to facilitate the off-site highways works to Manor Road and David Street which would provide a loading bay and deliver improvements for pedestrians, there would also be a contribution required for changes to Traffic Regulation Orders and compensation for the loss of on-street parking bays. Dropped kerbs and tactile paving would be

required for informal road crossing points, to connect pedestrian route desire lines in the area. Reinstatement of footways with full height kerbs for any redundant accesses would also be required. A planning condition would control the off-site highways works, and it is therefore considered that the proposal would meet Core Strategy Policies T1 and T2.

## **10.5 Flood risk and sustainable drainage**

10.5.1 The application has been supported by information which adequately demonstrates that the NPPF flood risk sequential test has been passed. The site is proposed for residential development however it is in flood zone 2 and therefore an exception test is not required.

10.5.2 The following flood mitigation measures are proposed in the submitted Flood Risk Assessment (FRA):

- All apartments would be provided at first floor level and above which will be significantly higher than the design flood level (1in100yr + climate change), the 1in1000yr flood level and the existing ground level.
- The ground floor would be occupied by a gym, cinema, cycle store, bin store and other ancillary uses and would be set at a minimum level of 28.4mAOD which is approximately 300mm higher than the existing ground levels (28.1mAOD) and allows for 400mm of freeboard above the 1in100yr + climate change flood level (27.996mAOD). This level is also less than 500mm below the 1in1000 yr flood level (28.866mAOD).
- The nature of the development would mean that residents could escape to the upper floors of the building if necessary in the event of a flood where they could remain until the flood waters recede. The applicant would also prepare a flood warning and evacuation plan, and sign up to Environment Agency flood alerts.
- External ground levels would be designed to direct any surface water flow away from building thresholds.

10.5.3 The applicant has submitted a sustainable drainage strategy (SuDS), which sets out the following measures:

- Post-development surface water run-off rate should be restricted to the greenfield run-off rate if practicable.
- Unless an area is designed to hold and/or convey water, flooding does not occur on any part of the site for a 1 in 30-year rainfall event.
- Flooding does not occur during a 1 in 100-year storm event (including an allowance for the anticipated impacts of climate change) in any part of a building (including a basement) or in any utility plant susceptible to water (e.g. pumping station or electricity substation) within the development.
- Flows resulting from rainfall in any event exceeding the 1 in 100-year rainfall event are managed in exceedance routes to minimise as far as practicable the risk of flooding to people and property both on and off site.
- The SuDS design for the development site should ensure that the quality of any receiving water body is not adversely affected and preferably enhanced.

10.5.4 The Environment Agency and Leeds City Council Flood Risk Management have no objection to the application proposal subject to conditions regarding the implementation of the measures set out in the flood risk assessment and details of the surface water drainage. It is therefore considered that the submitted flood risk and drainage proposal would comply with the requirements of the NPPF, Core Strategy policy EN5 and NRWDPD policies Water 4, 6, and 7.

## **10.6 Wind**

10.6.1 The applicant has submitted a quantitative wind report in support of the application, which has been independently peer reviewed by a qualified and experienced wind consultant on behalf of the Council. Based on the modelling provided, a potential safety concern has been highlighted at the entrance to the flats, because the safety threshold for entrances could be exceeded for vulnerable users in the windiest predicted conditions. In this case, it is likely that mitigation measures would be required to reduce this risk, such as the reconfiguration of the entrance ramp and steps. Discussions with the applicant are on-going at the time of writing. It is requested that Members defer and delegate approval to the Chief Planning Officer, in order to progress the satisfactory resolution of this matter.

## **10.7 Other considerations**

10.7.1 In relation to one of the objector's concern about lack of pre-application consultation with leaseholders, the planning application was publicised on 9<sup>th</sup> June 2017 with 3 site notices (Siddall Street, Manor Road and on the pedestrian link close to the residential entrance to Manor Mills), and a press notice in accordance with statutory requirements. Residents and leaseholders can also sign up to alerts on Public Access on the Council's website to be notified when planning applications are submitted in their area. In addition, the applicant's agent states that residents were notified by the agent at pre-application stage via leaflets in the entrance to the flats. It is very difficult to establish all possible land interests within a block of flats and consult with all potential freeholders, leaseholders and tenants, and therefore this is not standard practice for planning application publicity.

10.7.2 Whilst concerns regarding visual dominance, loss of daylight and outlook, and construction noise and disturbance are acknowledged, the present open aspect and lack of activity to the west enjoyed by Manor Mills is a temporary condition, pending redevelopment in accordance with longstanding adopted policy objectives and the future regeneration of the Holbeck/South Bank area. A condition is recommended to control construction activities, working and delivery hours, in order to minimise the potential for harm to amenities.

## **10.8 Planning obligations and Community Infrastructure Levy (CIL)**

10.8.1 A legal test for the imposition of planning obligations was introduced by the Community Infrastructure Levy Regulations 2010. These provide that a planning obligation may only constitute a reason for granting planning permission for the development if the obligation is:

- (a) necessary to make the development acceptable in planning terms,
- (b) directly related to the development; and
- (c) fairly and reasonably related in scale and kind to the development.

10.8.2 The proposal is for Private Rented Sector (PRS) flats. The National Planning Policy Guidance set out above makes a distinction for PRS provision which states that: 'The economics of such schemes differ from build to sale and should be determined on a case by case basis. To help ensure these schemes remain viable while improving the diversity of housing to meet local needs, local planning authorities

should consider the appropriate level of planning obligations, including for affordable housing, and when these payments are required’.

10.8.3 It is accepted that PRS housing has specific characteristics, brought about through its funding model, which may make it unsuitable for the delivery of on-site Affordable Housing provision through a Housing Association or Registered Provider (RP). The applicant has proposed to pursue three options in turn starting with on-site provision with a Housing Association through to an off-site contribution if on-site provision is not possible. In some PRS schemes it has been possible to secure on site provision of affordable flats to be managed directly by the PRS provider at the Council’s benchmark rents, in perpetuity, with compliance to a Local Lettings Policy. If this option is not possible, there may be a commuted sum payable, with no flats to be sold on for at least 10 years from completion. The flats would be provided in a pro-rata mix of one, two and three bed flats. This will be controlled through the S106 agreement. This is considered to be an acceptable approach given the March 2017 Executive Board report.

10.8.4 Adopted policies are likely to result in the following necessary Section 106 matters:

- Affordable Housing 5% in a pro-rata mix of flats in a tiered approach starting with
  - (i) on-site provision managed by a Housing Association,
  - (ii) on-site provision managed by the PRS provider in accordance with the Council’s benchmark rents and a Local Lettings Policy in perpetuity, and
  - (iii) a commuted sum based on the Council’s benchmark rents as set out in the March 2017 Executive Board report
- Sustainable Travel Fund £14,803
- Car club trial provision £10,000
- Amendments to traffic regulation order to replace 3 pay and display bays to accommodate loading and car club £7,500
- Revenue compensation for loss of 3 pay and display bays £18,000
- Travel plan monitoring fee £2505
- Public access to routes at ground level
- Cooperation with local jobs and skills initiatives

10.8.5 The proposal would be subject to the Community Infrastructure Levy (CIL) and this has been calculated as £30,731.47

## **11.0 Conclusion**

11.1 This scheme is a significant regeneration opportunity that will contribute to the delivery of the Council’s adopted place-making vision for the transformation of the Holbeck area of Leeds South Bank, and high quality development which will bring with it a number of new sustainable dwellings. The scheme would provide a range of house types and sizes, and provide construction jobs. The proposed redevelopment would also enhance the character of the surrounding area, by regenerating a prominently-located unsightly and under-utilised brownfield site that has been vacant for many years. Section 66(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990 states that Local Planning Authorities (LPAs) must have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses and pay special attention to the desirability of preserving or enhancing the character and appearance of a Conservation Area, and it considered that the proposal would enhance the setting and significance of the Grade I listed building Temple Mill and the character and appearance of the nearby Holbeck Conservation Area. The proposal is on balance in accordance with the Development Plan and national planning policy as described above.

**Background Papers**

Application file 17/02666/FU

**Appendix 1**

Draft Conditions for 17/02666/FU

**Appendix 2**

Proposed Site Layout Plan



## Appendix 1

### Draft Conditions for 17/02666/FU

- 1) The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Imposed pursuant to the provisions of Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

- 2) The development hereby permitted shall be carried out in accordance with the plans listed in the Plans Schedule

For the avoidance of doubt and in the interests of proper planning.

- 3) The approved Phase I Desk Study report indicates that a Phase II Site Investigation is necessary, and therefore development shall not commence until a Phase II Site Investigation Report has been submitted to, and approved in writing by, the Local Planning Authority.

Where remediation measures are shown to be necessary in the Phase II Report and/or where soil or soil forming material is being imported to site, development shall not commence until a Remediation Statement demonstrating how the site will be made suitable for the intended use has been submitted to, and approved in writing by, the Local Planning Authority. The Remediation Statement shall include a programme for all works and for the provision of Verification Reports.

To ensure that the presence of contamination is identified, risks assessed and proposed remediation works are agreed in order to make the site 'suitable for use'.

- 4) If remediation is unable to proceed in accordance with the approved Remediation Statement, or where significant unexpected contamination is encountered, the Local Planning Authority shall be notified in writing immediately and operations on the affected part of the site shall cease. An amended or new Remediation Statement shall be submitted to, and approved in writing by, the Local Planning Authority prior to any further remediation works which shall thereafter be carried out in accordance with the revised approved Statement.

To ensure that any necessary remediation works are identified to make the site suitable for use.

- 5) Remediation works shall be carried out in accordance with the approved Remediation Statement. On completion of those works, the Verification Report(s) shall be submitted to the Local Planning Authority in accordance with the approved programme. The site or phase of a site shall not be brought into use until such time as all verification information has been approved in writing by the Local Planning Authority.

To ensure that the remediation works are fully implemented as agreed and the site has been demonstrated to be suitable for use.

- 6) Prior to the commencement of development, a Statement of Construction Practice shall be submitted to and approved in writing by the Local Planning Authority. The Statement of Construction Practice shall include full details of:

- a. the methods to be employed to prevent mud, grit and dirt being carried onto the public highway from the development hereby approved;
- b. measures to control the emissions of dust and dirt during construction;
- c. location of site compound and plant equipment/storage;
- d. location of contractor and sub-contractor parking;
- e. how this Statement of Construction Practice will be communicated by the developer to local residents
- f. Construction works and delivery hours shall be restricted to 0800-1800 hours Monday to Friday, 0800-1300 hours on Saturdays, with no works on Sundays and Bank Holidays.

The approved details shall be implemented at the commencement of work on site, and shall thereafter be retained and employed until completion of works on site. The Statement of Construction Practice shall be made publicly available for the lifetime of the construction phase of the development in accordance with the approved method of publicity.

In the interests of residential amenity of occupants of nearby property

- 7) The development permitted by this planning permission shall be carried out in accordance with the approved Flood Risk Assessment (FRA) reference no: 17028, Manor Road Leeds, Flood Risk Assessment & SuDS Statement, dated April 2017 and the following mitigation measures detailed within the FRA:
  - a. Ground floor finished levels shall be set no lower than 28.4m above Ordnance Datum (AOD).
  - b. There shall be no sleeping accommodation on the ground floor.
  - c. Safe routes into and out of the site to an appropriate safe haven shall be provided.
  - d. preparation of a flood risk warning and evacuation plan

To reduce the risk of flooding to the proposed development and future occupants, reduce the risks of flooding to occupants in a flood event, and to ensure safe access and egress from and to the site.

- 8) Building operations shall not commence until a drainage scheme (i.e. drainage drawings, summary calculations and investigations) detailing the surface water drainage works has been submitted to and approved in writing by the Local Planning Authority. The surface water discharges should be restricted to 5 l/s unless otherwise otherwise agreed in writing with the Local Planning Authority and surface water drainage scheme shall be accord with the Leeds City Councils Minimum Development Control Standards for Flood Risk. The works shall be implemented in accordance with the approved scheme before the development is brought into use.

To ensure sustainable drainage and flood prevention.

- 9) No piped discharge of surface water shall take place until works to provide a satisfactory outfall for surface water have been completed in accordance with details to be submitted to and approved by the local planning authority before development of that phase commences.

To ensure that the site is properly drained and surface water is not discharged to the foul sewerage system which will prevent overloading.

- 10) Prior to the commencement of building works, a sample panel of all external facing materials, roofing and glazing types to be used shall be constructed on-site and approved in writing by the Local Planning Authority. The external cladding and glazing

materials shall be constructed in strict accordance with the sample panel(s) which shall not be demolished prior to the completion of the development.

In the interests of visual amenity.

- 11) No building works shall be commenced until full 1 to 20 scale working drawing details of the following have been submitted to and approved in writing by the Local Planning Authority:
- a. soffit, roof line and eaves treatments
  - b. junctions between materials
  - c. each type of window bay proposed
  - d. ground floor frontages

The works shall be carried out in accordance with the details thereby approved.

In the interests of visual amenity and the character of the surrounding area.

- 12) No surfacing works shall take place until details and samples of all surfacing materials have been submitted to and approved in writing by the Local Planning Authority. Such materials shall be made available on site prior to the commencement of their use, for the inspection of the Local Planning Authority who shall be notified in writing of their availability. The surfacing works shall be constructed from the materials thereby approved and completed prior to the occupation of the building.

In the interests of visual amenity in accordance with Leeds Core Strategy Policy P12, Saved UDP Review Policies GP5 and LD1, and the NPPF.

- 13) Landscaping works shall not commence until full details of both hard and soft landscape works, including an implementation programme, have been submitted to and approved in writing by the Local Planning Authority. Hard landscape works shall include
- a. proposed finished levels and/or contours,
  - b. boundary details and means of enclosure,
  - c. other vehicle and pedestrian access and circulation areas,
  - d. hard surfacing areas,
  - e. minor artefacts and structures (e.g. furniture, play equipment, refuse or other storage units, signs, lighting etc.),
  - f. proposed and existing functional services above and below ground (e.g. drainage, power cables, communication cables, pipelines etc., indicating lines, manholes, supports etc.).

Soft landscape works shall include

- g. planting plans
- h. written specifications (including soil depths, soil quality, tree pits, cultivation and other operations associated with plant and grass establishment) and
- i. schedules of plants noting species, planting sizes and proposed numbers/densities.

All hard and soft landscaping works shall be carried out in accordance with the approved details, approved implementation programme and British Standard BS 4428:1989 Code of Practice for General Landscape Operations. The developer shall complete the approved landscaping works and confirm this in writing to the Local Planning Authority prior to the date agreed in the implementation programme.

To ensure the provision and establishment of acceptable landscape in accordance with Leeds Core Strategy Policy P12, Saved Leeds UDP Review (2006) policies GP5, N23, N25 and LD1.

- 14) A landscape management plan, including long term design objectives, management responsibilities and maintenance schedules shall be submitted to and approved in writing by the Local Planning Authority prior to the occupation of the development. The landscape management plan shall be carried out as approved.

To ensure successful aftercare of landscaping.

- 15) If within a period of five years from the date of the planting of any tree/hedge/shrub that tree/hedge/shrub, or any replacement, is removed, uprooted or destroyed or dies, or becomes, in the opinion of the Local Planning Authority, seriously damaged or defective, another tree/hedge/shrub of the same species and size as that originally planted shall be planted in the same location as soon as reasonably possible and no later than the first available planting season, unless otherwise agreed in writing by the Local Planning Authority.

To ensure maintenance of a healthy landscape scheme

- 16) No building works shall take place until details for the provision of off-site highways works shown on the submitted plan reference 17027-GA-01-C have been submitted to and approved in writing by the Local Planning Authority for inclusion in the section 278 Highways Agreement or to be secured by such other procedure as may be agreed between the applicants and the Local Planning Authority. Works shall be completed prior to the occupation of the development, unless otherwise agreed in writing by the Local Planning Authority.

In the interests of pedestrian and vehicular safety.

- 17) Notwithstanding the details shown on the plans hereby approved and prior to the commencement of building works, full details of the facilities for the parking of cycles for residents shall be submitted to and approved in writing by the Local Planning Authority. The development shall not be brought into use until the cycle parking facilities thereby approved have been provided. The facilities shall thereafter be retained and maintained as such.

In in the interest of promoting sustainable travel.

- 18) Prior to the installation of any extract ventilation system or air conditioning plant, details of such systems shall be submitted to and approved in writing by the Local Planning Authority. Any external extract ventilation system/air conditioning plant shall be installed and maintained in accordance with the approved details. The systems shall limit noise to a level at least 5dBA below the existing background noise level (L90) when measured at the nearest noise sensitive premises including flats within the development with the measurements and assessment made in accordance with BS4142:1997.

In the interests of visual and residential amenity.

- 19) No building works shall commence until a detailed acoustic assessment of the proposed building and a noise insulation scheme, including details of any necessary mechanical ventilation equipment, have been submitted for formal written approval by the Local Planning Authority. The scheme shall be designed to protect the amenity of

the residents of the proposed dwellings from noise from surrounding roads, adjoining uses and any plant or equipment associated with the approved building. The noise insulation scheme for the development shall be designed to achieve internal noise levels in living spaces not exceeding 35dB(A) and 30dB(A) in bedrooms at night, with peak levels kept below 45dB(A). The approved scheme shall be implemented prior to occupation of the dwellings and retained thereafter

In the interests of residential amenity

- 20) Prior to the commencement of building operations an updated Sustainability Statement shall be submitted to and approved in writing by the Local Planning Authority for that phase, which shall include a detailed scheme comprising:

- a. a recycled material content plan (using the Waste and Resources Programme's (WRAP) recycled content toolkit)
- b. a Site Waste Management Plan (SWMP)
- c. an energy plan showing the amount of on-site energy produced by the selected Low and Zero Carbon (LZC) technologies and that it produces a minimum of 10% of total demand, unless otherwise agreed in writing by the Local Planning Authority
- d. details that demonstrate a minimum of 20% carbon dioxide emissions reduction against Part L of the 2013 Building Regulations and meet the Low Water Usage Target

The development shall be carried out in accordance with the details as approved above;

- e. Within 6 months of the occupation of the development a post- construction review statement shall be submitted by the applicant including formal accreditation and approved in writing by the Local Planning Authority

The development and buildings comprised therein shall be maintained and any repairs shall be carried out all in accordance with the approved detailed scheme and post-completion review statement or statements.

To ensure the adoption of appropriate sustainable design principles

- 21) Prior to first occupation a scheme detailing the method of storage and disposal of litter and waste materials, including recycling facilities, shall be submitted to and approved in writing by the Local Planning Authority. The details shall include a description of the facilities to be provided including, where appropriate, lockable containers and details of how the recyclable materials will be collected from the site with timescales for collection. The approved scheme shall be implemented before the development hereby permitted is brought into use and no waste or litter shall be stored or disposed of other than in accordance with the approved scheme.

In the interests of amenity and to promote recycling.

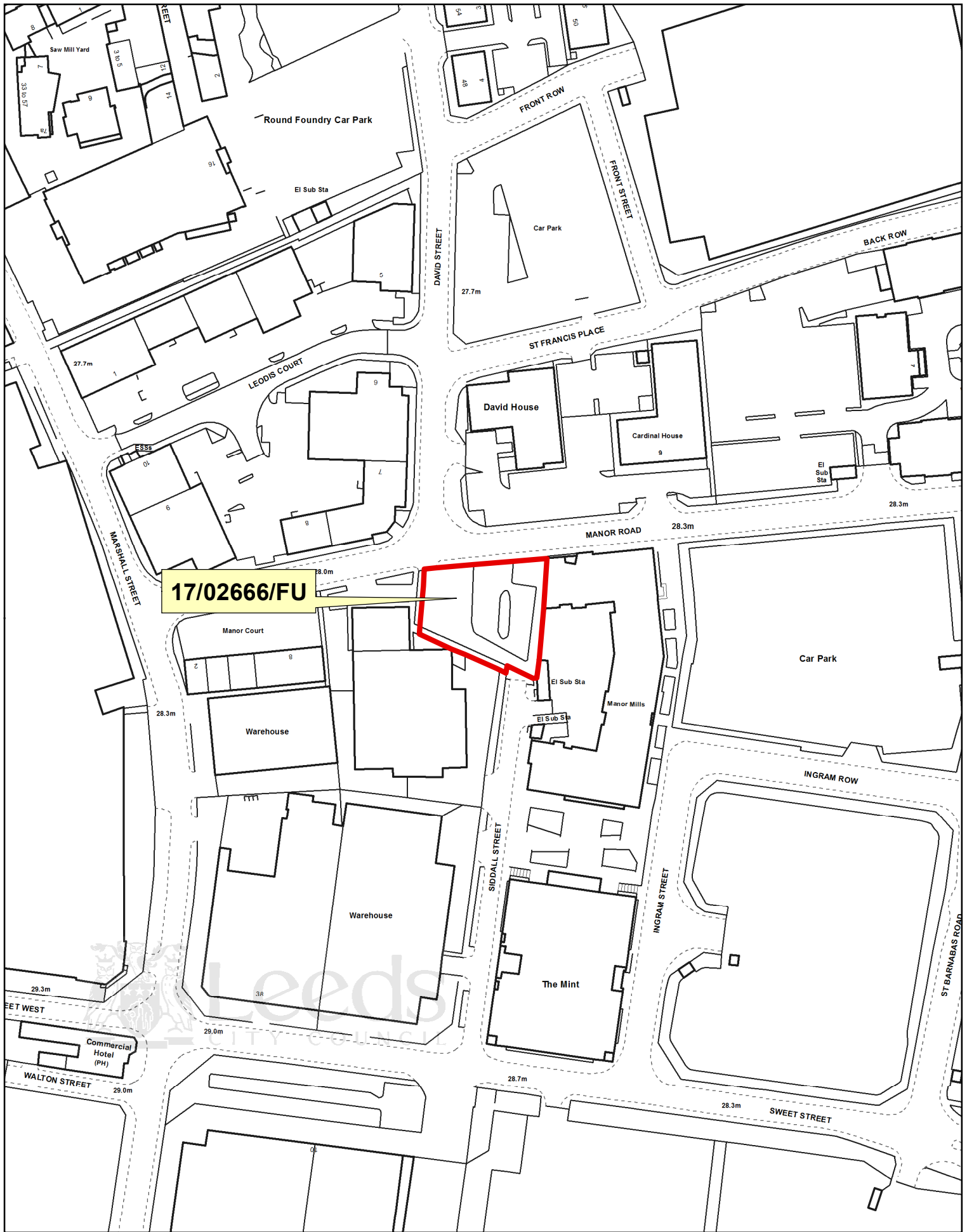
- 22) Notwithstanding details shown on the plans hereby approved the gradient of pedestrian ramps shall meet BS8300:2009+A1:2010. Works shall be carried out in accordance with the approved details and retained as such thereafter.

In the interests of accessibility for all.

- 23) No works to or removal of trees or shrubs shall take place between 1st March and 31st August inclusive, unless a competent ecologist has undertaken a careful, detailed check of vegetation for active birds nests immediately before (within 24

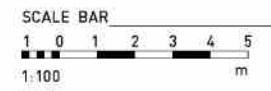
hours) the works commence and provided written confirmation that no birds will be harmed and/or that there are appropriate measures in place to protect nesting bird interest on site. Any such written confirmation should be submitted to the Local Planning Authority within 3 days of such works commencing.

To protect nesting birds in vegetation and built structures in accordance with the Wildlife and Countryside Act 1981 (as amended) and BS 42020:2013.



# CITY PLANS PANEL



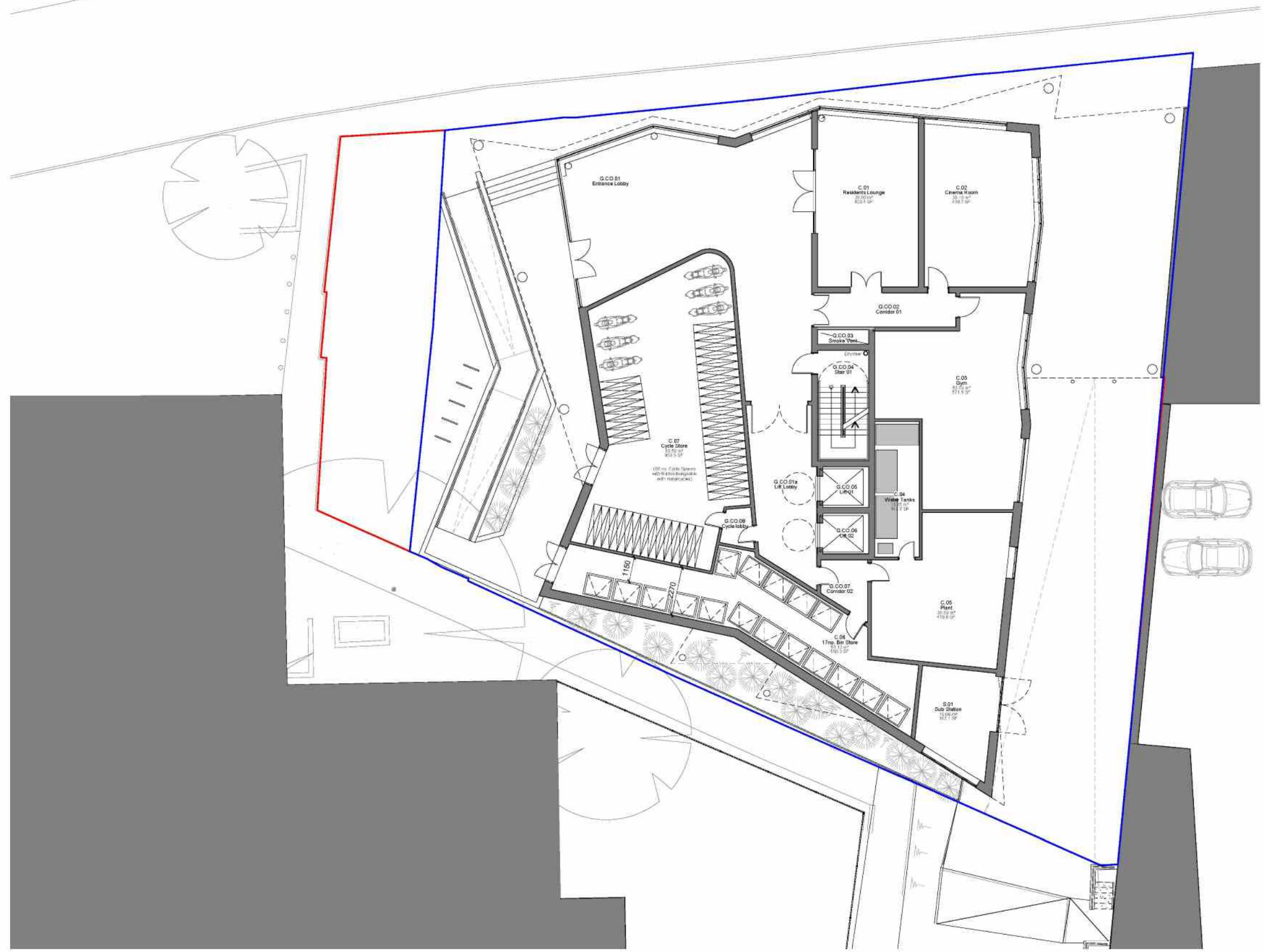


NOTE :  
Do not scale from this drawing, use figured dimensions only. Scale bar shown for reference only.

NORTH POINT



Application boundary  
Ownership boundary



Rev	Description	Date	Dr	App
G	Red line amended	20.09.17		
F	Entrance and cycle store amended for wind mitigation	18.09.17		
E	Ownership line amended	26.04.17		
D	Cycle store and ramp amended as highway consultant comments	24.04.17		
C	Ramp wall amended	03.4.017		
B	Site context added	24.02.17		PC
A	Room no's added	17.02.17		

original by	Author	date created	02/14/17	Approved by	Approver
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project  
**Proposed Residential Development for Citylife, Manor Road, Leeds**

drawing title  
**Proposed Ground Floor GA Plan**

project number  
**2016-107**

drawing number	P200	revision	G
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scale	1 : 100	title status	Prelim
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NOTES  
 Contractor must verify all dimensions on site before commencing any work on this drawing.  
 Check all dimensions before commencing work on site. If this drawing contains the quantities taken on site, the architect can be held responsible for any errors.  
 This drawing is intended to be used for the design of the building. It is not to be used for any other purpose.  
 Drawing based on: Citylife Leeds - Leeds City Council Planning and Building Department - 2016-107  
 Date: 02/14/17  
 Drawn by: Nick Brown  
 Checked by: Nick Brown  
 Approved by: Nick Brown  
 Scale: 1:100  
 Title: Proposed Ground Floor GA Plan  
 Project: Proposed Residential Development for Citylife, Manor Road, Leeds





Originator: Richard Smith

Tel: 2478000

## Report of the Chief Planning Officer

### *CITY PLANS PANEL*

Date: 2<sup>nd</sup> Nov 2017

Subject: PREAPP17/00517 – Residential development, associated facilities with linked podium

At Doncaster Monk Bridge Whitehall Road, Lower Wortley Leeds LS12 1BE

### APPLICANT

BAM Monk Bridge Ltd

#### Electoral Wards Affected:

City and Hunslet

Yes

Ward Members consulted

#### Specific Implications For:

Equality and Diversity

Community Cohesion

Narrowing the Gap

**RECOMMENDATION:** This report is brought to Plans Panel for information and comment. The developer will present the details of the scheme to allow Members to consider and comment on the proposals at this stage.

### 1.0 INTRODUCTION:

1.1 This is a pre-application presentation to seek Members' feedback on emerging plans for a multi-storey residential development on a site south of the listed viaduct which crosses through the former Doncaster Monkbridge Ironworks site off Whitehall Road in the City Centre. Members' comments are sought prior to the submission of any Full planning application.

### 2.0 SITE AND SURROUNDINGS:

2.1 The site forms part of the wider land holding owned by the applicant south of the grade II listed viaduct, which was formerly part of the wider Doncaster Monkbridge Iron & Steel works.

- 2.2 The site is located to the southwest of the commercial core of the city centre but within the defined city centre boundary. The Leeds Liverpool Canal is to the east of the site with the working railway line to the west. There is a mix of residential, commercial and industrial activities in the surrounding area.
- 2.3 The listed viaduct to the north was constructed in 1846 for the Leeds and Thirsk Railway Company. The viaduct has not been in use since the 1960s when the former Wellington train station closed and was subsequently demolished. It is in separate ownership.
- 2.4 One office building and access roads to serve the commercial plots south of the viaduct have been constructed and connected to Whitehall Road as part of previous planning permissions for the wider site but otherwise the site is cleared of all buildings.
- 2.5 The area is surrounded by a mixture of residential apartment blocks, the river and the canal, cleared land and office developments along Whitehall Road. The general theme of architecture along Whitehall Road is modern and contemporary, aside from the historic viaduct.
- 2.6 This particular section of the applicant's ownership has been previously granted Outline planning permission for 2 blocks of office development (5 and 6 stories in height), part of an overall group of 5 office blocks. Aside from Phase Red (to the opposite corner of the wider site) these have not been built out and the section of land forms part of a phased development which is now termed Phase Purple B.
- 2.7 The site is currently unallocated within the designated City Centre in the saved Unitary Development Plan Review Proposals Map. Within the emerging Local Development Framework Site Allocations plan, the Doncaster Monkbridge site is identified as a mixed use site (MX1-11).
- 2.8 The site lies in flood risk zone 2 (medium probability).

### **3.0 PROPOSAL**

- 3.1 The proposal is for 2 blocks of residential development consisting of 17 and 21 stories set either side of a raised area of shared landscaped private amenity space. The development would total 463 residential apartments which would be specifically built for rental purposes (Private Rented Scheme or 'PRS'). The development would be retained by the applicant and managed by a Specialist Provider. The apartments have a main pedestrian entrance set on the south side of the development.
- 3.2 The proposal includes parking space for 132 cars at ground and basement levels as well as ancillary gym, cycle storage space and concierge space.
- 3.3 The buildings would be set around an area of landscaped public realm with tree planting, seating and grassed / planted areas.
- 3.4 The accommodation would consist of the following:
- 79 x Studios (17%)
  - 151 x 1-bedroom apartments (33%)
  - 210 x 2-bedroom apartments (45%)

- 23 x 3-bedroom apartments (5%)

- 3.5 The blocks would be identical in external materials and appearance aside from the height difference. This would consist of a brick structure with recessed windows creating shadow lines and a grid pattern. The roof tops would be flat with the addition of two terrace gardens to the southern section of the top floor. These are communal spaces available to residents to book / hire sporadically throughout the year.
- 3.6 The style of the architecture has emphasis on verticality with horizontal banding. The entrances at ground floor level will be of double height, like that seen on the proposals elsewhere within the built and approved office buildings within the site.
- 3.7 Parking space is available within the ground floor and basement via ramped access provision which is reached through the southern side in between the two blocks and underneath the central landscaped (private) first floor level amenity space.
- 3.8 The applicant is proposing a total of 464 cycle parking spaces (one cycle space to each apartment) in a communal facility at ground level. A communal gym space is available to all residents at this level. The level of parking space (132 spaces including 16 disabled and 14 electric) amounts to 28.5% of the total number of apartments. There are also 15 motorcycle spaces proposed.
- 3.9 The amenity space is mainly hard surfacing treatment – decking, walkways, benches with some soft planting in raised planters and trees secured through tree pit design, to give some shading and softening within the surroundings.
- 3.10 Externally landscaped designs include new benches and further trees. This would be part of the wider public realm and connects with the proposed hard surfacing set in front of the listed viaduct and commercial units expected as part of the Foundation development to the north side of the Doncaster Monkbridge site.
- 3.11 The development is expected to generate planning obligations in the form of:
- Sustainable Travel Plan Fund contribution of £113,701.23
  - Travel Plan together with Monitoring Fee (£4,315)
  - City Car Club contribution of £20,000 to create car club space within the development / perimeter of site (deducted from cost of RTPF)
  - Traffic Regulation Order contribution towards access road restrictions and possibly on surrounding streets, dependent on the outcome of a Transport Assessment to be undertaken
  - Use of local employment skills in construction
- 3.12 In respect of Affordable Housing, the applicant is undergoing discussions with the Council at the time of writing. Initial calculation has indicated that 24 ‘affordable’ units (5% of the total number) will be required. As part of the proposed business model the applicant suggests this will be also managed by the Residential Management Company. These would probably consist of 12 x one-bed; 11 x 2-beds and 1/2 x three-beds.

#### 4.0 RELEVANT PLANNING HISTORY

- 4.1 The site has a complex planning history and the key applications are outlined below with a brief summary provided for each.
- 4.2 06/02880/OT: Outline application to layout access and erect multi-level mixed use development for residential and office uses up to 33 storeys high, with ancillary class A1, A2, A3, A4, A5, D1 and D2 uses and associated car parking and landscaped areas, approved 10<sup>th</sup> September 2007.

This is the main application that relates to the wider Doncaster Monkbridge site (i.e. that to the north and south of the viaduct and the viaduct itself). This outline consent granted permission for five office buildings to the south of the viaduct (up to 12 storeys high), works to the viaduct to introduce commercial uses in the arches and a landscaped area on top plus four residential towers of 16, 23, 29 and 33 storeys providing a total of 720 apartments to the north of the viaduct. The principle of development, means of access and siting of the buildings were agreed and a detailed design code set the design principles and scale of the buildings. Both the residential and commercial office elements of this remain extant.

- 4.3 The following elements of the Outline consent have been progressed:

06/05718/FU: Laying out of access road and erection of 8 storey office block, with basement car parking and rooftop plantroom, approved 10<sup>th</sup> September 2007; 'Phase Red' – fronting Whitehall Road – built and occupied

08/03199/RM: Laying out of pocket park with landscaping, approved 28<sup>th</sup> October 2008. As required by the outline consent, the pocket park between the canal and river was approved and subsequently laid out.

13/02017/RM: Reserved matters application for 10 storey office block with basement car park and roof top plant room, approved 12<sup>th</sup> October 2015. 'Phase Yellow' – also fronting Whitehall Road and yet to be built out.

17/05182/RM: Reserved matters application for 8 storey office building fronting the Leeds – Liverpool canal totalling 8 storeys yet to be built out. 'Phase Purple A' - this pending application is submitted for amended exterior designs further to pre-application presentation made to City Plans Panel in January 2017).

- 4.4 To the north of the viaduct, permission has recently been granted for the Foundation scheme, a development totalling 607 apartments split between Private Rented Sector (PRS) and private residential sale together with public realm and restoration improvements to the listed viaduct. This was presented to City Plans Panel in May 2017 with the applications approved in September 2017.

## **5.0 HISTORY OF NEGOTIATION AND ENGAGEMENT**

- 5.1 Meetings have been held between the agent and architect with planning, highways, design, landscaping and housing officers during September / October 2017.

- 5.2 City and Hunslet Ward Councillors were consulted on 9<sup>th</sup> October 2017.

## **6.0 RELEVANT PLANNING POLICIES**

### **6.1 Statutory Context**

**6.1.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004** requires the application to be determined in accordance with the development plan unless material considerations indicate otherwise.

**6.2 The Planning (Listed Buildings and Conservation Areas) Act 1990 (as amended)**

6.2.1 Section 66. In considering whether to grant planning permission for development which affects a listed building or its setting, the local planning authority.....shall have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses.

**6.3 The Development Plan**

6.3.1 For the purposes of decision making, the Development Plan for Leeds currently comprises the following documents:

1. The Leeds Core Strategy (Adopted November 2014)
2. Saved UDP Policies (2006), included as Appendix 1 of the Core Strategy
3. The Natural Resources & Waste Local Plan (NRWLP, Adopted January 2013)
4. Any Neighbourhood Plan, once Adopted

6.3.2 These development plan policies are supplemented by supplementary planning guidance and documents.

6.3.3 The policy guidance in Annex 1 to the National Planning Policy Framework (NPPF) is that due weight should be given to relevant policies in existing plans according to their degree of consistency with the NPPF.

**6.4 Leeds Core Strategy 2014**

6.4.1 The Core Strategy sets out strategic level policies and vision to guide the delivery of development investment decisions and the overall future of the district. The most relevant policies are set out below:

Spatial Policy 1 prioritises the redevelopment of previously developed land within Main Urban Area, in a way that respects and enhances the local character and identity of places and neighbourhoods.

Spatial Policy 3 seeks to maintain and enhance the role of the City Centre as an economic driver for the District and City Region, by comprehensively planning the redevelopment and re-use of vacant and under-used sites for mixed use development and areas of public space; enhancing streets and creating a network of open and green spaces to make the City Centre more attractive and improving connections between the City Centre and adjoining neighbourhoods.

Policy CC1 outlines the planned growth within the City Centre for 10,200 new dwellings, supporting services and open spaces. Part (b) encourages residential development, providing that it does not prejudice town centre functions and provides a reasonable level of amenity for occupiers.

H4 states that developments should include an appropriate mix of dwelling types and sizes to address needs measured over the long-term taking into account the nature of the development and character of the location.

H5 states that the Council will seek affordable housing from all new developments either on-site, off-site or by way of a financial contribution if it is not possible on site. For the City Centre, applications with 15 or more units should provide 5% of the total units as affordable units.

P10 requires new development to be based on a thorough contextual analysis to provide good design appropriate to its scale and function, delivering high quality innovative design and enhancing existing landscapes and spaces.

P11 refers to heritage assets which will be conserved and enhanced (including any nationally significant industrial heritage). Innovative and sustainable construction which integrates with and enhances the historic environment will be encouraged. Enabling development may be supported in the vicinity of historic assets where linked to the refurbishment or repair of heritage assets.

T1 and T2 identify transport management and accessibility requirements to ensure new developments are adequately served by highways and public transport and with safe and secure access for pedestrians, cyclists and people with impaired mobility. Sustainable travel planning and parking policies are also outlined within this.

EN1 and EN2 set out the sustainable construction and on-going sustainability measures for new development.

Policy EN4 outlines that where technically viable and appropriate for the development, and in areas with sufficient existing or potential heat density, developments of 1,000m<sup>2</sup> or more or 10 dwellings or more should look to utilize district heating systems where possible.

EN5 details how the Council will manage and mitigate flood risk including:

(ii) Requiring flood risk to be considered for all development commensurate with the scale and impact of the proposed development and mitigated

(iii) Reducing the speed and volume of surface water run-off (new build)

## **6.5 Leeds Unitary Development Plan Review 2006 (UDPR) Saved Policies**

### **6.5.1 Relevant Saved Policies include:**

BD2 - New buildings should complement and enhance existing skylines, vistas and landmarks.

BD5 - A satisfactory level of amenity for occupants and surroundings should be provided.

LD1 - Sets out criteria for landscape schemes.

The eastern part of the site is within the Waterfront Strategy Area as designated by the UDPR (2006). This strategy seeks to enhance the waterfront.

## **6.6 Leeds Natural Resources and Waste DPD 2013**

### **6.6.1 The plan sets out where land is needed to enable the City to manage resources, e.g. minerals, energy, waste and water over the next 15 years, and identifies specific actions which will help use natural resources in a more efficient way.**

## **6.7 Site Allocations Plan (SAP)**

- 6.7.1 In May 2017 the Council submitted the Site Allocations Plan (SAP) to the Secretary of State for independent Examination. The hearing sessions commenced on 24 October 2017.
- 6.7.2 The Aire Valley Area Action Plan (delivering circa. 7,000 homes) has also been subject of Examination in Public in January 2017 and consultation on Main Modifications in May 2017. All documents form part of the Council's up to date Local Plan.
- 6.7.3 This site is identified in the Publication Draft of the Site Allocations Plan as part of a larger site that also includes the land to the south of the viaduct. This site is identified as being able to deliver 463 units & 50,380 sqm of offices in Phase 1 (MX1-11).

## **6.8 Relevant Supplementary Planning Guidance includes:**

Affordable Housing SPG

Parking SPD

Tall Buildings Design Guide SPD – States that this site is within a 'string' of sites that would be suitable locations for tall buildings (evidenced by the City Island).

Travel Plans SPD

Building for Tomorrow Today: Sustainable Design and Construction SPD

Street Design Guide SPD

City Centre Urban Design Strategy

Leeds Waterfront Strategy SPG

## **6.9 Material Planning Considerations**

### **6.10 National Planning Policy Framework (NPPF)**

- 6.10.1 The NPPF sets out the Government's planning policies for England and how these are expected to be applied, only to the extent that it is relevant, proportionate and necessary to do so. The following paragraphs are considered most relevant:

6 and 7: sets out that planning should be committed to achievement of sustainable development and that the system should perform three key roles: economic, social and environmental.

17: It identifies 12 core planning principles which include that planning should:

- Proactively drive and support sustainable economic development to deliver homes....and thriving local places. Every effort should be made objectively to identify and then meet the housing, business and other development needs of an area.
- Plans should take account of market signals, such as land prices and housing affordability
- Recognize that residential development can play an important role in ensuring the vitality of centres
- Seek high quality design and a good standard of amenity for existing and future occupants.
- Conserve and enhance the natural environment
- Encourage the effective use of land by reusing land that has been previously developed (brownfield land)

- Promote mixed use developments and encourage multiple benefits from the use of land in urban areas.
- Actively manage patterns of growth to make the fullest possible use of public transport, walking and cycling and focus significant development in locations which are, or can be, made sustainable.

49: states that housing applications should be considered in the context of the presumption in favour of sustainable development.

50: states that local authorities should deliver a wide choice of homes widen opportunities for home ownership and create sustainable, inclusive and mixed communities (based on current / future demographic and market trends and different groups in the community).

56: Government attaches great importance to design of the built environment.

58: policies and decisions should aim to ensure developments:

- function well and add to the overall area quality over the long term
- establish strong sense of place, creating attractive, comfortable places
- optimise potential of site to accommodate development
- respond to local character and history
- create safe and accessible environments
- are visually attractive (architecture and landscaping)

61: Visual appearance and architecture of individual buildings are important factors alongside connections between people and places, integration of new development into natural, built and historic environment.

95: To support the move to a low carbon future, local planning authorities should:

- plan for new development in locations and ways which reduce greenhouse gas emissions;
- actively support energy efficiency improvements to existing buildings; and
- when setting any local requirement for a building's sustainability, do so in a way consistent with the Government's zero carbon buildings policy and adopt nationally described standards.

## **6.11 National Planning Practice Guidance (NPPG)**

6.11.1 This provides Central Government Guidance on a range of planning matters and provides the following advice underneath the Viability section:

### The private rented sector

“Some privately rented homes can come from purpose built schemes held in single ownership which are intended for long term rental. The economics of such schemes differ from build to sale and should be determined on a case by case basis. To help ensure these schemes remain viable while improving the diversity of housing to meet local needs, local planning authorities should consider the appropriate level of planning obligations, including for affordable housing, and when these payments are required. So these homes remain available to rent only, Local Planning Authorities may choose to explore using planning obligations to secure these schemes for a minimum period of time. Local Planning Authorities should enforce these planning obligations in the usual way.”

## **6.12 Other Material Considerations**



6.12.1 On the 22 March 2017 Leeds City Council's Executive Board endorsed an approach which recognises that the acceptance of commuted sums from BTR schemes may be appropriate and justified in accordance with Core Strategy Policy H5.

6.12.2 The Leeds Standard and the DCLG Technical Housing Standards

6.12.3 The Leeds Standard sets out the importance of excellent quality housing in supporting the economic growth ambitions of the Council. The Leeds Standard sizes closely reflects the Government's Technical Housing Standards – Nationally Described Space Standard which seek to promote a good standard of internal amenity for all housing types and tenures. Whilst neither of these documents has been adopted as formal planning policy in Leeds given their evidence base in determining the minimum space requirements they are currently used to inform decisions on the acceptability of development proposals. The Council has committed to prepare a Development Plan Document (DPD) which will allow the national standards to be applied to new housing development in Leeds. This is programmed to be incorporated within the Core Strategy selective review, with public consultation taking place later this year.

## 7.0 CONSULTATION RESPONSES

Environment Agency: Site lies within flood zones 1 and 2. Standing Advice applies: Flood Risk Assessment will be required. Sequential Test will be required.

Network Rail: No objection but has requirements which must be met given the proximity of an electrified railway line:

- Residential amenity should be carefully considered given the proximity of the railway including adequate sound proofing over a 24hr period.
- Drainage should be directed away from the railway. Any attenuation to assist surface water drainage systems should be undertaken to the advice of specialist engineers.
- Directions including reference to the safe use of cranes, excavations and earthworks, safe working practice including piling within proximity of the railway and scaffolding.
- Security fencing along the boundary. The fence needs to be trespass-proof
- NR wish to be involved in the design and species selection of landscaping and lighting design.
- Glint and glare issues should be analysed in the design of the development to eliminate any unsafe reflections of the sun at various times during the day.

### 7.1.2 Non-Statutory:

Contaminated Land Team: Phase 1 Desk Top Study required.

Flood Risk Management: Site lies with flood zones 1/2. Soils data suggest infiltration SuDS (sustainable drainage systems) may be difficult to accommodate on the site. Likely the site will therefore need to drain to the public sewer, where discharge rates will need to be based upon greenfield rates of discharge. Pre development enquiry with Yorkshire Water recommended prior to any application.

Highways Services: Transport Assessment required which should consider the operation of the site access with Whitehall Road and also the junction with Globe Road with Whitehall Road. It should also consider connections to public transport, cycling and walking. Access roads within the should be made adoptable and brought

up to suitable standards (including pedestrian width of footways). Adequate servicing arrangements should be provided which do not block sight lines of other drivers. Car parking arrangements on the ground floor need widening in parts. Provision for drop off / collection / deliveries should be provided. Cycle and parking numbers are in accordance with the Parking SPD. Car park and servicing management plan should be provided. Travel Plan will be required. Traffic Regulation Orders (TROs) will be required on the estate roads and potentially on surrounding streets (dependent on the outcomes of the Transport Assessment).

Sustainability – Landscape Officer: Consideration should be given to rainwater harvesting for the roof gardens and street trees. Co-ordination in respect of the routes of services should be undertaken in respect of the landscaping. Trees will require load bearing soil cells to be protected and nourished.

Sustainability - Nature Conservation: Up to date bat roosting surveys should be carried out to any viaduct arches immediately adjacent to the site (along north-west boundary). Bat roosting features have been proposed to be installed into outside parts of the adjacent viaduct as part of application 16/07714/FU (Foundation) and will require lower light levels below these features; the Latitude scheme should demonstrate this will be deliverable.

Travelwise: A full residential Travel Plan should be provided. This should explain how it links to travel plan implementation for the existing offices and the wider site. Car club provision will be expected, along with a travel plan fund and travel plan review fee.

## **8.0 MAIN ISSUES**

Members are asked to comment on the current proposals and to consider the following matters:

### **8.1 Principle of the Uses**

8.2 The site forms part of a wider mixed use allocation in the Site Allocations Plan – ref. MX1-11 (Inner Area). This includes the land to the north of the listed viaduct subject of the recent approved ‘Foundation’ residential development scheme. The allocation notionally details the site as to contain 463 residential units and 50,380m<sup>2</sup> of office development. This number of residential units is lower than the 2006 Outline consent (726) and the recent Foundation scheme (607). The proposal would increase the density of development to provide 1,070 residential units.

8.3 The further increase in residential use of the Doncaster Monkbridge allocation / site is considered acceptable; it is sustainably located in the City Centre, offers good public transport, cycling and walking connections including how the site can link into the viaduct connections and the city, shown and detailed within the Foundation scheme.

8.4 The increase in density of development to this site is also expected to assist with the ongoing vitality of the listed viaduct which will look to accommodate small commercial units contributing to future use and upkeep of this key connection into the City Centre.

8.5 The residual loss of office space will still leave some 35,633m<sup>2</sup> of office space (71% of the allocation figure). In the context of the wider City Centre, there is a significant provision of office accommodation following a number of permissions over recent years including a significant coverage across the neighbouring MEPC site. The

Doncaster Monkbridge site still has capacity to provide a further 8 storey (Phase Purple A) and 10 storey (Phase Yellow) office buildings behind and to the side of the existing occupied 8 storey (Phase Red) office building.

8.6 Given the site location and sustainability characteristics, the increase in residential capacity of the site is considered suitable under policies CC1 and T2. Given the level of B1a employment space approved already within the City Centre, the reduction by under a 1/3 of this site's provision is considered on balance acceptable as part of its mixed use allocation.

8.7 **Do Members support the proposed residential use of the site?**

8.8 Design, Massing and Protection of the Special Character and settings of Listed Buildings

8.9 The proposal would feature two tall buildings in the foreground of the listed viaduct. The development would be seen in the background of some other listed buildings in the area such as The Roundhouse and Half Roundhouse. The applicant has produced a Heritage Statement which concludes that the development would have a 'low adverse' impact upon the listed viaduct of which less than substantial harm will occur (reference to the NPPF).

8.10 The statement outlines that the original Monkbridge Ironworks would have afforded little in the way of historic clear views to this impressive structure. Resultantly the public benefits of the scheme in supporting the future long term protection of this asset through sustainable re-use is considered to outweigh the level of harm generated from the building blocks set in front of part of the structure. The Heritage Statement is considered a reasonable and realistic assessment.

8.11 The vertical shaping and massing of the blocks is designed to integrate with the wider masterplan of the site and should not significantly alter views into and through to the listed viaduct over and above the siting and footprints of the 5 and 6 storey blocks of the previous office consents.

8.12 The vertical designs also assist with provision of the views through to the horizontal shaping of the listed viaduct. The simple rectangular blocks would sit in line with the surrounding blocks built and previously approved on the southern side of the viaduct.

8.13 The design and massing has been considered in respect of both the existing and future context of the site with seven modelled views taken from strategic points around the development.

8.14 In terms of the existing context, there are some notable tall buildings within the City scape, including City Island at up to 15 stories tall in parts. Other examples further along the riverside include Candle House and further on, Bridgewater Place at 32 stories.

8.15 Given the surrounding context which includes the River Aire and Leeds-Liverpool Canal, Whitehall Road and the operational railway lines it is considered by Officers that there is sufficient 'breathing space' for a tall development of 17 to 21 stories high. The Tall Buildings Design Guide (2010) recognises that the wider Doncaster Monkbridge was the focus of a permitted tall building as was the triangular shaped site to the south side of Whitehall Road (both over 30 stories). The former has now been superseded by the recent approval of the Foundation scheme with tall blocks up to 21 stories in height.

- 8.16 The design and siting would also be acceptable in the context of the Foundation scheme to the north of the viaduct which increases in mass towards the City Centre. Together with the yet unbuilt office blocks (Phases Yellow and Purple A) the development will be largely obscured in large parts by the emerging surrounding context.
- 8.17 The designs are considered to provide simple but effective vertical lines with a strong linear grid pattern which sits at ease and in juxtaposition adjacent to the horizontal and regular arched structure of the listed viaduct. The predominantly brick and glazed built elevations sit effectively in the surrounding area with the backdrop of City Island, the MEPC development and approved Foundation Scheme all using these same materials in different quantities.
- 8.18 Although refinement to the early drawings has been discussed, the broad design emphasis is supported by Officers under LDF policies P10 and P11.
- 8.19 Do Members consider that the scale of the proposed new buildings and their relationship with the surrounding context is acceptable?**

8.20 Housing Mix, Internal Design, Affordable Housing Provision

- 8.21 The site, although set within the designated City Centre Boundary, is also set within the Inner Area Housing Market Characteristic Area (as opposed to the City Centre HMCA). In beginning to undertake the background research to this, the applicants have therefore initially appraised the demographics of both areas. The scheme is 100% flats to which policy H4 outlines may be suitable in a certain urban context. The site is a busy area of Leeds City Centre which is subject to transport noise and surrounded by densely built development. It is not considered that family housing is appropriate in this context.
- 8.22 Both these areas have been found to have higher than average percentages of young adults (20-34 years old) than the district average; 38% compared to 25%.
- 8.23 The analysis has suggested that the profile of renters in these locations are often young professionals aged 20-34, likely to be co-habiting couples, within house shares, or one person households. Therefore the housing mix proposed comprises of the following (as detailed in bold) with reference to LDF policy H4:

Size	Maximum (%) – Leeds District wide	Minimum (%) – Leeds District wide	Target (%) - Leeds District wide	<b>Site (%)</b>
Studio/1-bed	50	0	10	<b>50</b>
2-bed	80	30	50	<b>45</b>
3-bed	70	20	30	<b>5</b>
4-bed	50	0	10	<b>0</b>

- 8.24 Aside from the 3-bed units, the scheme falls within the ratios recommended. It is more reliant on studio / 1-bed units, but this is reflected by the initial research undertaken as to the needs of the demographics. The profile of the demographics suggests that these resident groups will not demand large percentages of 3-bed units.
- 8.25 Under LDF policy H4, as a scheme comprising more than 250 units, a Housing Needs Assessment will be required for submission and analysis.

- 8.26 The size of the apartments is as follows:
- Studio apartments – 33m<sup>2</sup> each
  - One-bed apartments – 41m<sup>2</sup> each
  - Two-bed apartments – 63m<sup>2</sup> each
  - Three-bed apartments – 74m<sup>2</sup> each
- 8.27 The studios are set just below the Leeds Standard which for a studio unit sets a standard of at least 37m<sup>2</sup>.
- 8.28 The 1, 2 and 3-bed units meet the minimum nationally prescribed space standards.
- 8.29 The units come with the communal benefit of gym provision and foyer lounge areas at ground floor level in addition to external ‘private’ areas for resident use such as the podium space and top floor roof terrace gardens available to book.
- 8.30 The majority of the accommodation would meet the Nationally Described Housing Standards, with the exception of the studio flats (which are 33m<sup>2</sup>). However, it is considered that the proposed layout, daylight, circulation and juxtaposition of living functions in these flats together with the provision of communal spaces (which total some 2,500m<sup>2</sup>) would be satisfactory and on balance it is considered that these are acceptable.
- 8.31 The apartments are considered to provide sufficient outlook and light and sufficient separation distances between the two blocks of apartments (33m).
- 8.32 The applicant is offering 5% of the total units (which amounts to 24) on site as affordable units pepper-potted across the mix and the blocks. This has been proposed to be split pro rata evenly across all unit sizes. Further analysis and discussion between the Council and applicant on the split ratios are underway (based on up to date need) and any verbal update on this will be given if necessary.
- 8.33 The applicant has outlined that the affordable units would be operated by the same management company that will operate the whole development on behalf of the owner. The applicant has confirmed that the tenancy would be to the terms of the Local Lettings Agreement as signed and secured under legal agreement. This would consider and accept Local Authority nominations.
- 8.34 The applicant is proposing that affordable housing rental levels would be agreed with the Council at scheme specific rates based on the applicant’s own viability appraisals (not to date yet supplied to the Council) as opposed to using the Council’s PRS £/m<sup>2</sup> benchmarks. These would be discounted from the full agreed (as verified) market value priced apartments with figures set as discounted for lower quartile (40% of the units) and lower decile (60% of the units) occupants with reference to policy H5.
- 8.35 Do Members consider the mix of apartment sizes suitable based on initial research findings and local demographic characteristics?**
- 8.36 Do Members feel that the space within the apartments offers sufficient levels of amenity for future occupiers?**
- 8.37 Do Members have any particular comments to make with respect to the applicant’s affordable housing position?**

8.38 Highway Considerations

- 8.39 The Highway consultation recognises that the site is well placed for walking and cycling. The applicant offers a secure internal / lockable cycle space for every single apartment. Whitehall Road is an advisory cycle route and the canal offers attractive cycling links to and from the site in addition. The site will be able to connect to the stairway within and onto the listed viaduct to connect to the City Centre core. Pedestrian and cycle connectivity (including across Whitehall Road) however will be analysed as part of a detailed Transport Assessment (TA) submitted with any Full application.
- 8.40 The proximity of the site to bus stops on Whitehall Road also takes advantage of frequent and regular services into and out of the City.
- 8.41 The provision of car parking at 132 spaces amounts to 28.5% of the number of the units. This is considered appropriate given the location within the City Centre boundary and walking / cycling / public transport connectivity. This also meets the guidance of the Parking SPD which in this City Centre 'core' location refers to the Residential Street Design Guide, which states that it is expected that sites would generally average no more than 0.6 spaces per dwelling. Visitor parking is not normally appropriate in such instances. Sufficient disabled and electric spaces are included within this to the ratios set out in the SPD.
- 8.42 The existing estate spine roads require improvements including footpath widening to be brought up to adoptable standards. This is considered achievable and realistic and the applicant has confirmed this will be offered subject to detailed legal review and engagement with other landowners. The applicant has taken on this information and is investigating how this can be satisfactorily addressed.
- 8.43 Servicing arrangements require further design detailing to ensure sufficient and safe provision for longer vehicles as well as delivery vans etc are catered for without adversely affecting traffic circulation and sight lines. As part of this it is expected that turning arrangements will be provided.
- 8.44 A Travel Plan will be required and provision of a financial contribution at £113,701.23 to support the Sustainable Travel Plan Fund including the provision of free residential use of the City Car Club, where provision for a publically accessible car / space will be provided as part of the ongoing site development / layout designs.
- 8.45 Traffic Regulation Orders will be required to the estate roads and dependent on the outcome of the TA possibly within the surrounding road network.
- 8.46 Do Members support the proposed level of parking provision?**

8.48 Flood Risk Management

- 8.49 The site lies partially in Flood Zone 2 therefore a Flood Risk Assessment will be required together with a Sequential Assessment of sites. This will be done consistent to the parameters (area) used within the Foundation scheme, namely the Inner Area Housing Market Characteristic Area.
- 8.50 The site is designed with non-habitable accommodation at ground floor level. Detailed drainage design which has not yet been undertaken will need to be

informed by liaison with Yorkshire Water and Flood Risk Management. It is expected that the site will drain to greenfield run off rates.

8.51 Ecology

8.52 A bat survey will be required and external lighting will need to be designed in accordance with this study work in this area which has bat roosting potential.

8.53 The site is partially adjacent to a Habitat Area (watercourse corridor) where it is not expected to have significant impacts upon this.

8.54 Wind

8.55 A detailed wind study will be required within any Full application. The applicant has however in the meantime commissioned a Scoping Report to inform the contents of this Study. An initial wind report has been also drafted for consideration. These will need to be considered under peer review, discussions on this being undertaken have already begun. Detailed feedback will be given to Members at any Full application.

8.56 Landscaping

8.57 The landscaping scheme to the podium will give residents a private decked area to supplement to existing Pocket Park set between the river and canal which offers publically accessible open space secured under the terms of the original Outline consent (reference drawn to policy G5).

8.58 Set around the development additional greenery from pit planted trees is shown to the surrounding public realm areas. The Landscape Officer recommends that sufficient sized and designed cell systems are used to bed these in place which could be maintained from sustainable rainwater harvesting / drainage systems.

8.59 It is considered that there is sufficient balance between hard materials externally and soft landscaping.

8.60 Conclusions

8.61 Members are asked to note the contents of the report and the presentation, and are invited to provide feedback, in particular, on the issues outlined below:

**Do Members consider that the scale of the proposed new buildings and their relationship with the surrounding context is acceptable? (8.19)**

**Do Members consider the mix of apartment sizes suitable based on initial research findings and local demographic characteristics? (8.35)**

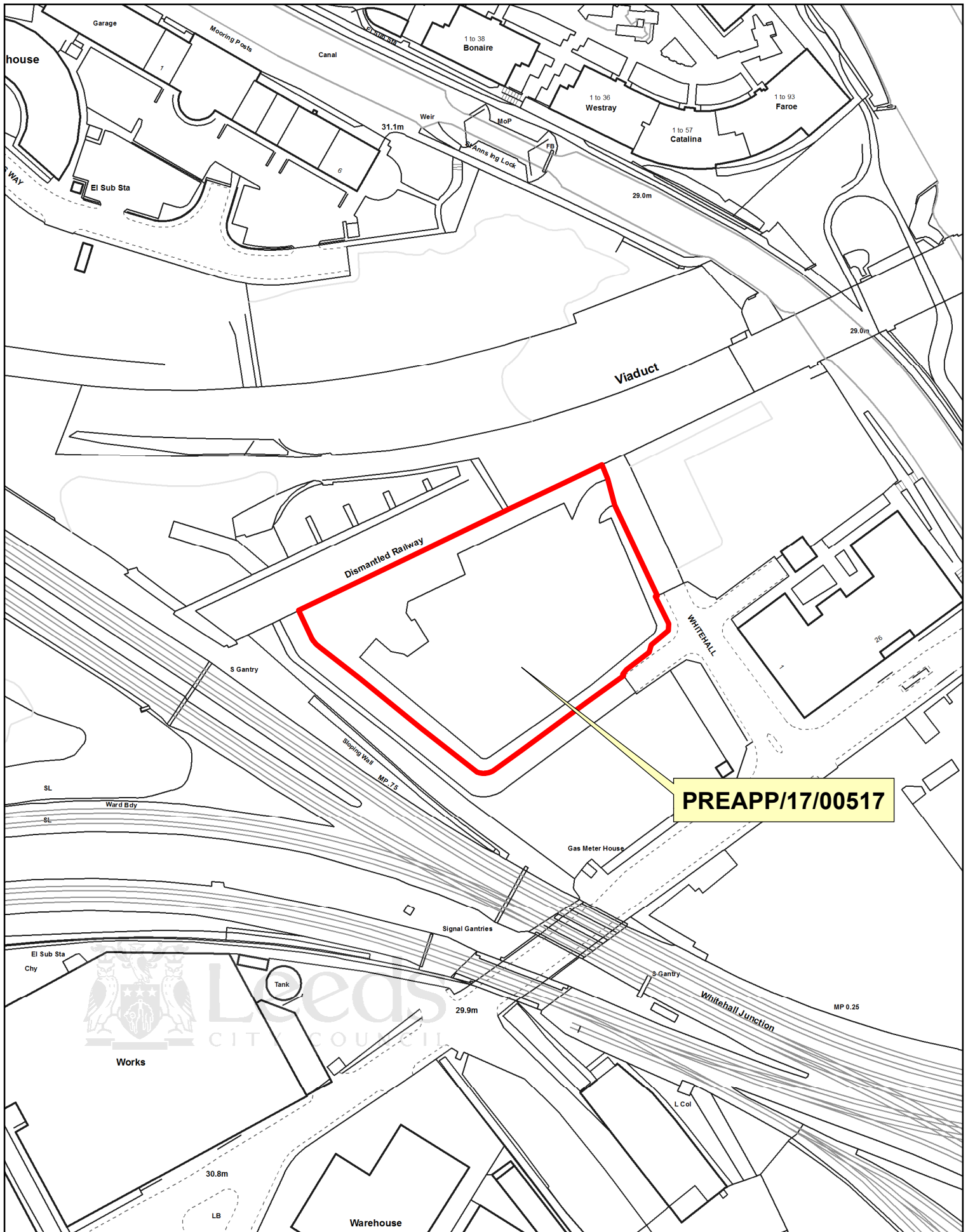
**Do Members feel that the space within the apartments offers sufficient levels of amenity for future occupiers? (8.36)**

**Do Members have any particular comments to make with respect to the applicant's affordable housing position? (8.37)**

**Do Members support the proposed level of parking provision? (8.46)**

Pre-application file: PREAPP/17/00517  
06/02880/OT: Outline application

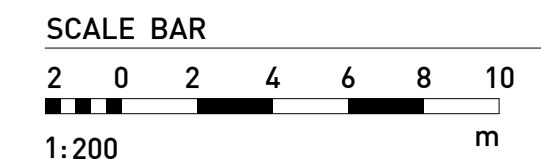




**PREAPP/17/00517**

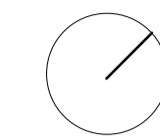
# CITY PLANS PANEL





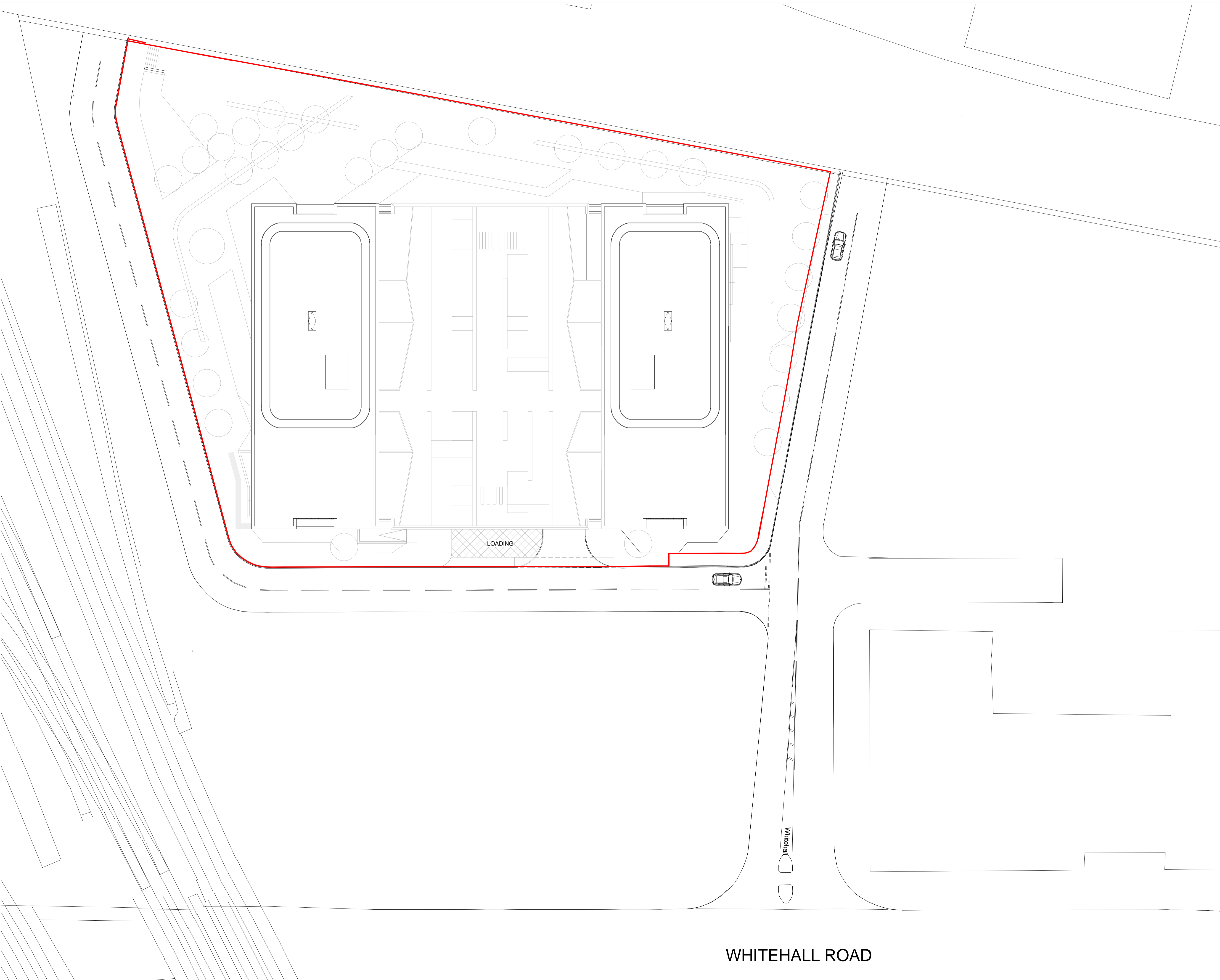
NOTE :  
Do not scale from this drawing, use figured dimensions only. Scale bar shown for reference only.

NORTH POINT



Key

— Nominal Red Line  
Boundary of Phase Purple  
B Site



B	Planning Issue	29.06.17	YA
A	Minor amends	09.06.17	
Rev	Description	Date	Dr by App by
original by	Author	date created	Approved by
		05/23/17	Approver

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project  
**Proposed Residential Development - Latitude Purple**

drawing title:  
**Site GA Plan**

project number  
**2016-108**

drawing number	revision
P110	B

scale:	issue status
1 : 250 GA1	

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WHITEHALL ROAD